

Type III Categorical Exclusion Action Classification Form

STIP Project No.	<u>U-5713, R-5777A & R-5777B</u>
WBS Element	<u>50111.1.FS1, 44648.1.2 & 44648.1.3</u>
Federal Project No.	<u>*NHPP-0070(182) [*U-5713 portion only]</u>

A. Project Description:

The proposed projects would upgrade U.S. 70 to interstate standards from the Neuse River Bridge to east of Thurman Road (S.R. 1116) in Craven County, North Carolina. State Transportation Improvement Program (STIP) U-5713 would construct improvements from Garner Road (S.R. 1121) to the Neuse River Bridge, a distance of approximately 2.5 miles. STIP R-5777A&B would construct improvements from east of Thurman Road to Garner Road, a distance of approximately 2.6 miles. For the purposes of this document, U-5713 and R-5777A&B are referred to as “the project.”

B. Description of Need and Purpose:

The project is one of several that NCDOT is working on to improve the U.S. 70/Future I-42 corridor, with the intent to improve regional mobility and provide better connectivity between Raleigh and the seaport at Morehead City, as well as to the military bases in Havelock and Goldsboro and the Global Transpark in Kinston. The 2015 Fixing America's Surface Transportation Act designated U.S. 70 from I-40 in Garner to the port at Morehead City as a high-priority corridor and future interstate (I-42). The intent of the I-42 designation is to benefit military interconnectivity, aid in economic development, create jobs and stimulate growth in travel and tourism. In addition, U.S. 70 is a designated hurricane evacuation route between the coast and the I-95 interchange in Johnston County.

The purpose of this project is to add additional capacity and access management improvements to the U.S. 70 corridor through James City, with the goal of providing local and regional benefits in traffic operations and economic development. In addition, the project will address local community concerns related to connectivity, access, and modal choice, including walking and biking.

C. Categorical Exclusion Action Classification: Type III

D. Proposed Improvements:

NCDOT Preferred Alternative – U-5713

For the U-5713 project, NCDOT proposes to add two additional travel lanes (one in each direction) along U.S. 70 from Garner Road (S.R. 1121) to the Neuse River Bridge. In addition, the project would close existing median openings and at-grade intersections, with interchanges and service road connections constructed to maintain access to the U.S. 70 corridor. The existing service roads would be extended in some locations and relocated away from the interchanges to maintain access to the side streets. The project would raise U.S. 70 over a re-connected Elder Street (S.R. 1138/S.R. 1917) and build interchanges to provide full access to U.S. 70 at Grantham Road (S.R. 1124), Airport Road, and Williams Road (S.R. 1167).

The project will also provide wider paved shoulders in some areas, and wider outside travel lanes in others, to accommodate cyclists and sidewalks along several cross streets that currently lack these facilities. New sidewalks would be provided along Grantham Road (S.R. 1124), Williams Road, Airport Road and Elder Street. James City residents would see

notable change in access across U. S. 70 with the removal of the existing pedestrian bridge over U.S. 70 in conjunction with the reconnection of Elder Street under U.S. 70, which will include sidewalks. Members of the public have raised concerns about the existing pedestrian bridge since its construction due to its appearance and the fact that bridge ramps and stairs are in isolated areas not connected to sidewalks, with the result that it is not widely used. The reconnection of Elder Street will provide a more accessible route across U.S. 70 for pedestrians.

The rail spur that crosses U.S. 70 at grade between Williams Road and the Neuse River Bridge will be removed as part of this improvement project. On-going coordination related to right of way acquisition will continue between NCDOT, North Carolina Railroad, Norfolk Southern, and the business that the spur line serves into the anticipated Design-Build phase of the project.

NCDOT Preferred Alternative – R-5777A&B

For the R-5777A&B project, no additional travel lanes would be added along U.S. 70 from Garner Road (S.R. 1121) to the eastern terminus of the R-5777A&B project; however, the proposed interchange improvements have been designed to be compatible with future widening of this section of U.S. 70.

Similar to the U-5713 project, the R-5777A&B project would close existing median openings and at-grade intersections, and maintain access with new interchanges and service road connections that would be constructed to maintain access to the U.S. 70 corridor. The existing service roads that terminate at Garner Road would be extended east through the entire R-5777A&B project area to maintain access to the side streets. The project would build interchanges to provide full access to U.S. 70 at Thurman Road (S.R. 1116) and Taberna Way (S.R. 1922).

The project would also construct new connecting roads to extend Taberna Way and Day Star Lane on new location between U.S. 70 and Old Cherry Point Road (S.R. 1113).

Other Alternatives Evaluated

In addition to the NCDOT Preferred Alternative, the following alternatives to the proposed improvements to U.S. 70 were considered:

- No-Build Alternative
- Alternate Modes of Transportation
- Transportation Management Alternative
- New Location Alternatives

The No-Build Alternative would not reduce congestion along U.S. 70 nor would improve regional mobility or access management.

The Alternate Modes of Transportation and Transportation Management alternatives could help reduce congestion on U.S. 70, but they would not provide the level of benefit of the Preferred Alternative nor would they improve connectivity between Raleigh and the seaport at Morehead City.

New location alternatives would have substantial impacts to both the human and natural environment and would not meet the purpose and need for the project because they would not provide the additional capacity needed through this portion of the U.S. 70 corridor, nor would they move enough traffic from this corridor to the new location corridor to reduce travel delay.

Cost estimates for the Preferred Alternative are provided in Table 1.

Table 1: Cost Estimates for the Preferred Alternative

	U-5713	R-5777 A&B
Right-of-Way Cost	\$33,428,561	\$23,615,521
Utilities Cost	\$12,722,028	\$ 7,685,347
Construction Cost	\$113,500,000	\$69,700,000
Total Costs	\$159,650,589	\$101,000,868

NOTES:

1. *Right of Way estimates dated June 25, 2018*
2. *Utility estimates dated May 2, 2018 (R-5777AB) and June 21, 2018 (U-5713)*
3. *Construction Cost estimates dated June 29, 2018*

Table 2: Impact Matrix for the Preferred Alternative

Resources		U-5713 Preferred Alternative	R-5777 A&B Preferred Alternative	Combined Total Preferred Alternative
Length (miles)		2.5	2.6	5.1
Relocations ¹	Residential	9	8	17
	Business	29	7	36
	Non-profit	0	1	1
Total Relocations		38	16	54
Minority / Low Income Populations (Disproportionate Impacts)		0	0	0
Historic Properties (Adverse Effects)		0	0	0
Community Facilities Impacted		0	0	0
Section 4(f) Impacts		0	0	0
Noise Receptor Impacts		30	32	62
Prime Farmland (acres)		0	0	0
Riparian Buffers (square feet)				
Zone 1		17,156	61,902	79,058
Zone 2		2,483	40,394	42,877
Streams (linear feet)				
Perennial		273	1,159	1,432
Intermittent		60	411	471
Wetlands (acres)		0	2.3	2.3
100-Year Floodplain and Floodway Impacts (acres) ⁴		0.2	0	0.2
Federally Protected Species:				
Sensitive joint-vech		No Effect	No Effect	No Effect
Atlantic sturgeon		No Effect	No Effect	No Effect
American alligator		Not Required	Not Required	Not Required
Red knot		No Effect	No Effect	No Effect
Leatherback sea turtle		No Effect	No Effect	No Effect
Red-cockaded woodpecker		No Effect	No Effect	No Effect
West Indian manatee		No Effect	No Effect	No Effect
Rough-leaved loosestrife		No Effect	No Effect	No Effect
Northern long-eared bat		MA-LAA	MA-LAA	MA-LAA

NOTES:

4. The number of relocations shown above are conservative estimates of a worst-case scenario for each project. A smaller number of relocations are likely after the implementation of avoidance and minimization measures developed during final design.
5. Impact quantities are based on construction limits plus an additional 25 feet.
6. 100-Year Floodplain and Floodway Impacts based on project construction limits.
7. MA LAA denotes "May affect –likely to adversely affect."

E. Special Project Information:

Funding

The U-5713 and R-5777A&B projects are being planned together and are scheduled to be let for construction together, although they have separate Planning & Engineering (PE) funding sources, with U-5713 using federal funds and R-5777A&B using state funds. This single federal Categorical Exclusion is being prepared to comply with the National Environmental Policy Act (NEPA) for U-5713 and the State Environmental Policy Act (SEPA) for R-5777A&B.

Public Involvement Summary

According to early outreach on the part of the project's public involvement team, resentment in the James City community still lingers following an earlier NCDOT project that included intersection improvements along with removal/closure of several intersections for streets that previously provided local access across U.S. 70. The 2010 James City Improvements Feasibility Study (NCDOT, 2010) was being developed at approximately the same time that the intersections were being closed, which created confusion about how and why the work was done. Many citizens and members of the business community feel that they were not listened to, and that their needs were not addressed when the recommendations for the feasibility study were developed. As a result, there has been some degree of lingering mistrust on the part of the community with regard to NCDOT's intentions for improvements to U.S. 70. To help ensure ample opportunity for community input, public involvement efforts for the project included targeted outreach to the James City Community, including individual meetings and small focus group meetings in 2015, and door to door outreach in coordination with corridor-wide public meetings in October 2015 and December 2016.

The purpose of the October 2015 and December 2016 corridor-wide public meetings was to provide an opportunity for citizens to come together collectively to share their thoughts and ideas regarding the projects. There were 226 participants that attended the October 2015 3-day public meetings. There were 235 participants (with 9 being repeat attendees) that attended the December 2016 3-day public meetings. Comments provided at these meetings were used to guide the development of the conceptual designs. In addition, during this outreach, residents and businesses expressed concerns that the initial project extent (limited to U-5713) did not extend to Taberna Way (S.R. 1922) and Thurman Road (S.R. 1116). This input, in consultation with local and federal officials, led to the inclusion of the R-5777A&B improvements with the construction of the U-5713 project.

Two focus group meetings were held January 16, 2018, to gather input from local businesses and residents at the south end of the project about conceptual designs. Many of the 41 business owners and 47 residents who attended the two meetings were aware of the proposed projects. Most of the concerns and questions raised were related to property access. Following the meetings, the proposed project designs were modified where feasible, to address comments received about property access.

In addition to the meetings described above, newsletters were sent to residents and businesses within the project study area in Fall 2016 and Fall 2017. The newsletters described the project, provided updated information on developments subsequent to the previous opportunities for public input, and included contacts for additional information.

Through this early and extensive public outreach, NCDOT was able to understand the concerns of the James City communities and businesses and, in turn, the communities and businesses understood the challenges and constraints that NCDOT had to work with in planning and developing the project. Through this collaborative process, NCDOT was able to successfully resolve the public's initial apprehension and concerns related to the proposed improvements by incorporating their input into the project development process. Because of

this, no substantial controversy or public opposition exists to the proposed project.

Additional Documentation

The project figures, State Historic Preservation Office effects determination forms, and Relocation Reports are appended to this document. The below technical reports and additional documentation can be found in the project file:

- Natural Resources Technical Report
- Public Involvement
- Community Characteristics Report
- Community Impact Assessment
- Indirect and Cumulative Effects Screening Report
- Architectural Survey Report
- Archaeological Survey Report
- Hazardous Materials Report
- Capacity Analysis Report
- Traffic Noise Analysis Report

F. Project Impact Criteria Checklists:

<u>Type III Actions</u>		Yes	No
<p>If the proposed improvement is identified as a Type III Class of Action answer all questions.</p> <ul style="list-style-type: none"> • The Categorical Exclusion will require FHWA approval. • If any questions are marked "yes" then additional information will be required for those question in Section G. 			
1	Does the project involve potential effects on species listed with the US Fish and Wildlife Service (USFWS) or National Marine Fisheries (NMFS)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2	Does the project result in impacts subject to the conditions of the Bald and Golden Eagle Protection Act (BGPA)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3	Does the project generate substantial controversy or public opposition, for any reason, following appropriate public involvement?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4	Does the project cause disproportionately high and adverse impacts relative to low-income and/or minority populations?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5	Does the project involve substantial residential or commercial displacements or right of way acquisition?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6	Does the project include a determination under Section 4(f)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7	Is a project-level analysis for direct, indirect, or cumulative effects required based on the NCDOT community studies screening tool?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
8	Is a project level air quality Mobile Source Air Toxics (MSAT) analysis required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9	Is the project located in anadromous fish spawning waters?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10	Does the project impact waters classified as Outstanding Resource Water (ORW), High Quality Water (HQW), Water Supply Watershed Critical Areas, 303(d) listed impaired water bodies, buffer rules, or Submerged Aquatic Vegetation (SAV)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
11	Does the project impact waters of the United States in any of the designated mountain trout streams?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12	Does the project require a U.S. Army Corps of Engineers (USACE) Individual Section 404 Permit?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
13	Will the project require an easement from a Federal Energy Regulatory Commission (FERC) licensed facility?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14	Does the project include Section 106 of the National Historic Preservation Act (NHPA) effects determination other than a no effect, including archaeological remains? Are there project commitments identified?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
15	Does the project involve hazardous materials and/or landfills?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
16	Does the project require work encroaching and adversely effecting a regulatory floodway or work affecting the base floodplain (100-year flood) elevations of a water course or lake, pursuant to Executive Order 11988 and 23 CFR 650 subpart A?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
17	Is the project in a Coastal Area Management Act (CAMA) county and substantially affects the coastal zone and/or any Area of Environmental Concern (AEC)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
18	Does the project require a U.S. Coast Guard (USCG) permit?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
19	Does the project involve construction activities in, across, or adjacent to a designated Wild and Scenic River present within the project area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

20	Does the project involve Coastal Barrier Resources Act (CBRA) resources?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<u>Type III Actions (continued)</u>		Yes	No
21	Does the project impact federal lands (e.g. USFS, USFWS, etc.) or Tribal Lands?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
22	Does the project involve any changes in access control?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
23	Does the project have a permanent adverse effect on local traffic patterns or community cohesiveness?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
24	Will maintenance of traffic cause substantial disruption?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
25	Is the project inconsistent with the STIP or the Metropolitan Planning Organization's (MPO's) Transportation Improvement Program (TIP) (where applicable)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
26	Does the project require the acquisition of lands under the protection of Section 6(f) of the Land and Water Conservation Act, the Federal Aid in Fish Restoration Act, the Federal Aid in Wildlife Restoration Act, Tennessee Valley Authority (TVA), Tribal Lands, or other unique areas or special lands that were acquired in fee or easement with public-use money and have deed restrictions or covenants on the property?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
27	Does the project involve Federal Emergency Management Agency (FEMA) buyout properties under the Hazard Mitigation Grant Program (HMGP)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
28	Is the project considered a Type I under the NCDOT's Noise Policy?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
29	Is there prime or important farmland soil impacted by this project as defined by the Farmland Protection Policy Act (FPPA)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
30	Are there other issues that arose during the project development process that effected the project decision?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

G. Additional Documentation as Required from Section F

Response to Question 1 – Threatened and Endangered Species

- Northern long-eared bat (NLEB) (*Myotis septentrionalis*)
The USFWS has developed a programmatic biological opinion (PBO) in conjunction with the Federal Highway Administration (FHWA), USACE, and NCDOT for NLEB in eastern North Carolina. The PBO covers the entire NCDOT program in Divisions 1-8, including all NCDOT projects and activities. The programmatic determination for NLEB is “**May Affect, Likely to Adversely Affect**”. The PBO provides incidental take coverage for NLEB and will ensure compliance with Section 7 of the Endangered Species Act for five years for all NCDOT projects with a federal nexus in Divisions 1-8, which includes Craven County, where TIP U-5713 and R-5777A&B are located. This level of incidental take is authorized from the effective date of a final listing determination through April 30, 2020.

Response to Question 5 – Displacements/Right of Way Acquisition

Based on preliminary designs, 17 residences and 36 businesses, and one non-profit will be displaced. Five (5) of the homes and five (5) of the businesses to be relocated are owned or occupied by minorities. The Relocation Reports are appended to this document for reference.

Sufficient right-of-way and easements will be acquired to accommodate the proposed improvements along U.S. 70 and the adjoining side roads and service roads. Additional right-of-way and easements are required along U.S. 70 to accommodate the proposed widening, with the majority of the right-of-way being acquired at the locations of the proposed interchanges and in areas where the proposed service roads will be located. Right-of-way will also be acquired between Taberna Way and Old Cherry Point Road to accommodate a new connection between U.S. 70 and Old Cherry Point Road, as well as between the service road east of the Thurman Road interchange and Old Cherry Point Road for similar purposes. The resulting impacts of residential and business displacements, although substantial, will not result in a significant impact to the community based on the proportion of residences and businesses being relocated due to the project.

Response to Question 7 – Indirect and Cumulative Effects Screening Report

An *Indirect and Cumulative Effects Screening Report* was completed for the project and can be found in the project file. The following paragraphs summarize the conclusions of this report:

Projects U-5713 and R-5777A&B propose to upgrade U.S. 70 to interstate standards on existing alignment, with an increase in capacity along approximately one half of the overall proposed project length. The project would convert the existing partial control of access to full control of access, with an anticipated reduction in travel time along the mainline, U.S. 70. Population and employment within the Future Land Use Study Area (FLUSA) are expected to grow, but at modest rates, and local planners anticipate that available land will be developed over time, whether or not the project is constructed. The City of New Bern and Craven County have plans and ordinances in place to effectively manage growth and development, while protecting natural resources within the FLUSA. The Indirect and Cumulative Effects Screening Tool (Table F-1) generated a score of 15 points out of 32, which indicates that a Land Use Scenario Assessment (LUSA) is not likely warranted for this project.

It is anticipated that the project will not notably contribute to cumulative impacts to environmental resources in the FLUSA. The potential for the degradation of water quality also exists through erosion and stream sedimentation. Any direct natural environmental impacts by NCDOT projects would be addressed by avoidance and minimization consistent with programmatic agreements with the natural resource agencies during the Permitting process.

The notable natural environmental features found within the FLUSA are provided multiple protections under State and Federal laws and local permitting ordinances as described above. With these existing regulations in place, the combination of past, current and future projects is expected to have a minor impact on notable environmental resources in the FLUSA. The cumulative effect of this project when considered in the context of other past, present, and future actions, and the resulting impact on the notable human and natural features, will not notably contribute to cumulative impacts to environmental resources in the FLUSA. Direct natural environmental impacts by NCDOT projects will be addressed by avoidance, minimization, or mitigation consistent with programmatic agreements with the natural resource agencies during the Permitting process. All future development will be required to follow local, state, and federal guidelines and permitting regulations.

Response to Question 10 – Impacted Waters

Streamside riparian zones within the study area are protected under provisions of the Neuse River Buffer Rules administered by the North Carolina Division of Water Resources. The table below presents anticipated buffer impacts of the project and specific mitigation amounts will be determined during the permitting stage of the project.

Buffer Impacts of Project (square feet)

Stream	Zone 1	Zone 2
U-5713		
SA*	14,148	1,166
SD	3,008	1,317
R-5777A&B		
SA	29,943	19,284
SI	14,694	9,712
SJ	17,265	11,398

*Also located in R-5777A&B project study area

Response to Question 12 – Individual Section 404 Permit

The proposed project is expected to impact 2.3 acres of wetlands and 1,903 feet of streams. Under the current Section 404 permitting requirements, it is expected the project will require an Individual Permit (IP). In general, the US Army Corps of Engineers Wilmington District issues an IP for projects that result in 0.5 acre or more of fill to Waters of the US or 300 linear feet or more of stream impacts or if the project is considered by the agency to be a major action. This permit requires a full public interest review, including public notices and coordination with involved agencies, interested parties, and the general public.

Response to Question 14 – Section 106 effects determination

- **Historic Architecture Resources**

For the U-5713 and R-5777A&B projects, a comprehensive architectural survey of the project study area was conducted in March 2016 and evaluated 222 resources, of which 13 warranted further investigation (Eligibility review July 2016 by HPO and NCDOT). In July 2016, research and evaluation of these 13 resources commenced and findings were presented in a January 2017 technical report. In March of 2017, HPO concurred that the following resources were eligible for listing in the National Register of Historic Places (NRHP):

U-5713 Resources

- Pilgrim Chapel Missionary Baptist Church (CV0396)
- Jones Chapel AME Zion Church (CV1902)
- Meadows Cemetery (CV2783)

HPO also concurred that the Mt. Shiloh Missionary Baptist Church (CV2050) retains the elements for which it was listed in the NRHP. HPO also agreed to the established and proposed NRHP boundaries for each property

R-5777A&B Resources

- New Bern Battlefield Site (CV2055)

A Determination of Effects Meeting was held on March 20, 2018, at which the NC-HPO, FHWA, and NCDOT concurred that the proposed projects would have the following:

- Pilgrim Chapel Missionary Baptist Church (CV0396) – **No Effect**
- Jones Chapel AME Zion Church (CV1902) – **No Effect**
- Meadows Cemetery (CV2783) – **No Effect**

- Mt. Shiloh Missionary Baptist Church (CV2050) – **No Effect**
- New Bern Battlefield Site (CV2055) – **No Adverse Effect with Environmental Commitments (see Section H of this document for those commitments)**

- **Archaeological Resources**

In a letter from the North Carolina Department of Cultural Resources, State Historic Preservation Office, dated May 28, 2015, the U-5713 project was found to have no known archaeological sites within the proposed project area. Based on the HPO's knowledge of the area, they concluded that it was unlikely that any archaeological resources that may be eligible for inclusion in the NRHP will be affected by the project; therefore, they **recommended that no archaeological investigation be conducted in connection with this project.**

In a letter from the NCDOT Archaeological Surveys Group, dated October 23, 2017, NCDOT archaeologists stated that the study area for the R-5777A&B project was very large in order to accommodate various alternatives for the proposed interchanges at Taberna Way and Thurman Road. Much of the study area has a low to moderate potential for archaeological sites and no archaeological survey was recommended for those low-potential areas. However, there are several areas that have a higher potential requiring investigation, including the New Bern Battlefield site.

In a subsequent letter from the NCDOT Archaeological Surveys Group, dated February 26, 2018, the NCDOT Archaeological Survey Group noted that the project study area had been reduced based on the refined project alternatives and made the following recommendations:

Recommend survey of the level, well-drained, undeveloped areas near water (as shown on the maps below). Determine whether the New Bern Battlefield earthworks are within the A.P.E. along the east side of US 70.

Based on these recommendations, and the determination that the New Bern Battlefield earthworks are located within the project A.P.E. and are anticipated to be impacted by the proposed improvements, NCDOT is conducting on-going archaeological investigations and expects to have the results of these investigations available for HPO review in July/August 2018. Commitments to continued archaeological surveys and coordination with HPO are included in Section H of this document.

Response to Question 15 – Hazardous Materials

Sites of concern were identified in the GeoEnvironmental Pre-Scoping Comments for U-5713 dated 9/12/2016, and R-5777A&B dated 10/27/2017, and are included in the project file. In the U-5713 project study area, a total of 24 sites of concern were identified within the project study area: 20 UST facilities, 1 dry cleaner and 3 other sites (including 1 mixed use industrial site, 1 marine manufacturer and 1 autobody shop). In the R-5777A&B project study area, a total of 12 sites of concern were identified: 11 possible UST facilities and 1 Superfund site.

The construction of the project is anticipated to be handled through the Design-Build process. As such, the Design-Build Team will be required to do the following:

After submittal of the right of way / 60% roadway design plans, the Design-Build Team shall hold a right of way consultation with the NCDOT's GeoEnvironmental staff, Transportation Program Management Director, and key Design-Build Team members. Sites of concern within the proposed right of way that are noted in the GeoEnvironmental Pre-Scoping Comments and 3/14/2018 Addendum, and any other sites identified by the Design-Build Team during the right of way consultation, shall be investigated by the Department. The Department shall require 90 days from the date of the consultation to investigate and provide Right of Way Recommendations. The Right of Way Consultation shall occur prior to

the Design Build Team making offers to purchase the right of way on these sites of concern. The Design-Build Team shall adhere to all Right of Way Branch procedures regarding the acquisition of contaminated property and any Right of Way Acquisition Recommendations provided by the Department.

After the parcels with identified contamination and/or underground storage tanks (USTs) are acquired and cleared of all existing above ground structural improvements, the Department shall remove all USTs from the right of way within 60 days of notification that the structures have been removed. Contaminated soil shall be removed by the Department to the limits necessary to complete the construction project. All remaining contaminated soil shall be left in place and undisturbed.

Response to Question 16 – Floodplain

There are no FEMA regulated floodplains impacted by the R-5777A&B project.

The proposed U-5713 project would impact 0.20 acres of the Zone AE floodplain (i.e., 100-year floodplain) within the construction limits. The Selected Alternative would not result in substantial encroachment to regulatory floodplains and are not expected to increase the extent or level of flood hazard risk. NCDOT will coordinate with the Federal Emergency Management Agency (FEMA) and local authorities in the final design stage of the project to ensure compliance with applicable floodplain management ordinances.

Response to Question 22 – Changes in Access Control

For the R-5777A&B project, full control of access will be acquired along U.S. 70, including the ramps and intersecting roads where new interchanges are being proposed (i.e., Taberna Way and Thurman Road). The full control of access along these intersecting roads will extend from the proposed interchange to the proposed service road intersections.

On the U-5713 project, the following full control of access (measured from the ramp centerlines) is proposed:

Williams Road Interchange

West side of U.S. 70 – to service road (approx. 315 feet)

East side of U.S. 70 – approx. 490 feet

Airport Road Interchange

West side of U.S. 70 – to service roads (approx. 320 feet)

East side of U.S. 70 – to Old Cherry Point Road (approx. 600 feet)

Grantham Road Interchange

West side of U.S. 70 – to service road on south side of Grantham Road (approx. 355 feet)

East side of U.S. 70 – to Old Cherry Point Road (approx. 270 feet)

Response to Question 24 – Maintenance of Traffic

The proposed modifications to U.S. 70 would be constructed within an existing roadway network; as such, temporary delays associated with construction are anticipated. All construction operations would be scheduled to keep traffic delay minimized. All safeguards, safety devices, protective equipment, and any other actions reasonably necessary to protect the life and health of employees on the job, the safety of the public, and the property connected with performance of the work would be applied.

The construction of the project is anticipated to be handled through the Design-Build process. As such, the Design-Build Team will be required to maintain traffic as required in the TMP scope of work and other components of the RFP that relate to traffic maintenance. Adhering to North Carolina's standards will also be required for this project.

Response to Question 28 – Traffic Noise

Based on the preliminary Traffic Noise Report (TNR), there will be 62 noise receptor impacts as a result of the proposed improvements (30 impacts associated with U-5713 and 32 impacts associated with R-5777A&B). This evaluation partially completes the highway traffic noise requirements of 23 CFR 772. The TNR, once approved, will identify areas where noise abatement is likely --- that is, where it preliminarily meets feasibility and reasonableness criteria. Upon approval of the TNR, the results of that report, including updated impacts and the likely abatement locations, will be made available via the U-5713 Project website (www.ncdot.gov/projects/US70_JamesCity/) and will also be mentioned in an upcoming project newsletter that will be sent prior to the final round of public meetings later in 2018. Additionally, hard copies of the TNR will be available for review during these public meetings and Traffic Noise experts will be in attendance to address any questions or concerns from the public.

Final noise wall recommendations will be based on the analysis done during final design and reported in the Design Noise Report. The final decision on installation of noise abatement shall be made upon completion of project final design, the public involvement process, and compliance with NCDOT Policy.

H. Project Commitments

Craven County
US 70 Corridor Improvements
From the Neuse River Bridge to East of Thurman Road (S.R. 1116)
Federal Project No. *NHPP-0070(182) [*U-5713 portion only]
WBS Nos. 50111.1.FS1, 44648.1.2 & 44648.1.3
STIP Nos. U-5713, R-5777A & R-5777B

Hazardous Materials (NCDOT Division 2, Design-Build Team)

- Sites of concern within the proposed right of way that are noted in the GeoEnvironmental Pre-Scoping Comments and 3/14/2018 Addendum, and any other sites identified by the Design-Build Team during the right of way consultation, shall be investigated by the Department. The Department shall require 90 days from the date of the consultation to investigate and provide Right of Way Recommendations. The Right of Way Consultation shall occur prior to the Design Build Team making offers to purchase the right of way on these sites of concern. The Design-Build Team shall adhere to all Right of Way Branch procedures regarding the acquisition of contaminated property and any Right of Way Acquisition Recommendations provided by the Department.

After the parcels with identified contamination and/or underground storage tanks (USTs) are acquired and cleared of all existing above ground structural improvements, the Department shall remove all USTs from the right of way within 60 days of notification that the structures have been removed. Contaminated soil shall be removed by the Department to the limits necessary to complete the construction project. All remaining contaminated soil shall be left in place and undisturbed.

FEMA Floodplains and Floodways (NCDOT Division 2, Design-Build Team)

- This project involves construction activities on or adjacent to a FEMA-regulated stream(s). Therefore, the Division will submit sealed as-built construction plans to the NCDOT Hydraulics Unit upon completion of project construction, certifying the drainage structure(s) and roadway embankment located within the 100-year floodplain were built as shown in the construction plans, both horizontally and vertically.

NC Floodplain Mapping Program (NCDOT Division 2, Design-Build Team, NCDOT Hydraulics Unit)

- NCDOT will coordinate with the NC Floodplain Mapping Program (FMP), the delegated state agency for administering FEMA's National Flood Insurance Program, to determine the status of the project with regard to applicability of NCDOT'S Memorandum of Agreement, or approval of a Conditional Letter of Map Revision (CLOMR) and subsequent final Letter of Map Revision (LOMR).

Reconnection of Elder Street/Removal of Existing Pedestrian Bridge/Addition of Sidewalks (NCDOT Division 2, Design-Build Team)

- The designs for both projects have been developed to accommodate input from the James City Environmental Justice (EJ) community, which has been impacted by transportation projects implemented over the last several decades that separated neighborhoods. The following commitments have been incorporated into the designs to mitigate the impacts to the surrounding communities:

- a new bridge on U.S. 70 over Elder Street that allows for a reconnection of Elder Street
- dismantling the existing pedestrian overpass bridge
- inclusion of sidewalks along both sides of the following roadways:
 - Elder Street
 - Williams Road
 - Airport Road
 - Grantham Road

These designs need to be retained through the Design-Build process and into the construction phase to avoid adverse and disproportionate impacts to the James City community.

Minimum Horizontal Clearance (NCDOT Division 2, Design-Build Team)

- To mitigate the impacts created by the proposed retaining walls and interchanges, the proposed designs will incorporate wider openings to allow for bicycles and pedestrians to pass underneath. A minimum of 14 feet from the edge of travel to the front of the barrier or end bent wall will be required under bridges for this project. The six bridges are:
 - Elder Street
 - Williams Road
 - Airport Road
 - Grantham Road
 - Taberna Way
 - Thurman Road

Removal of the Norfolk Southern At-Grade Spur Line Railroad Track (NCDOT Division 2, NCDOT Rail Division, Design-Build Team)

- NCDOT will continue to coordinate with the North Carolina Railroad, Norfolk Southern, and the Dixie Chemical Company regarding the removal of the at-grade rail spur that crosses U.S. 70 in the vicinity of the Neuse River Bridge. This at-grade crossing will be removed as part of the proposed project.

Cultural Resources (NCDOT Archaeological Group, Division 2, Design-Build Team)

- *New Bern Battlefield Earthworks (CV2055)*
Project activities within the existing right of way on the east side of U.S. 70 will impact the earthworks, however a finding of *no adverse effect* will be established when the commitments detailed below are fulfilled:
 1. Construction activities will not impact any land outside (east of) the existing ROW (all land within 35 meters (115 feet) of the U.S. 70 pavement).
 2. Archaeological survey will be conducted within the existing ROW from the earthworks north to a small drainage valley, a distance of approximately 273 meters (900 feet). The survey will consist of the excavation of shovel tests at a 15-meter (50-foot) interval within the existing ROW.
 3. A metal detector survey will be conducted within the existing ROW along the east side of U.S. 70 from the earthworks north to a small drainage valley. The metal detector survey will be followed by shovel tests placed at each positive reading.
 4. The earthworks will be examined by excavation of a trench across it. The cross-section will be recorded with a measured drawing and photographs.
 5. The results of the survey/testing will be provided in a report that will be reviewed by NCDOT and HPO/OSA staff.

6. No staging or storage of equipment and materials will occur within the National Register boundary and corresponding vicinity of the newly identified earthworks section.
 7. Tree protection will be installed along the ROW during construction.
 8. To ensure avoidance of the earthworks, protective fencing will be installed defining its boundary. A member of the NCDOT Archaeology Group (Caleb Smith, 919-707-6086) will assist with the installation of this protective fencing.
- *On-going Archaeological Surveys*
NCDOT will conduct the required archaeological survey for R-5777AB in several areas where high archaeological potential has been identified along each side of U.S. 70.

Continued Coordination and Outreach (NCDOT Division 2, Design-Build Team)

- *School Buses*
Because 23 school buses utilize U.S. 70 and intersecting streets, making 46 trips each school day, it is recommended that the Design Build Team coordinate with Craven County Schools (252-514-6377) prior to any construction that may result in substantial delays for school buses.
- *Craven Area Regional Transit*
It is possible that Craven Area Regional Transit (CARTS) will be operating a fixed route transit system that serves the entire New Bern Urbanized Area by the time that construction is scheduled to begin. Therefore, it is recommended that the Design Build Team contact CARTS (252-636-4917) prior to construction to determine if service is being provided within the project study area, and if so, coordinate regarding detours and construction delays that may impact service.
- *Business Outreach and Bike MS*
It is recommended that the Design Build Team coordinate with the New Bern Area Chamber of Commerce (252-637-3111) regarding outreach to businesses in the project study area, and to the Bike MS organizers (FundraisingSupport@nmss.org) prior to project construction.
- *Fire/EMS Outreach*
It is recommended that the Design Build Team coordinate with local emergency service providers, including No. 7 Township Fire and Rescue (252-637-2779) and Craven County Emergency Services (252-636-6608), regarding potential detour routes, and prior to any construction that may result in substantial delays for emergency vehicles.
- *Marine Corps Air Station at Cherry Point*
U.S. 70 is a designated STRAHNET route and serves military commuters. Because the project is expected to have temporary impacts on mobility during construction, it is recommended that the Design Build Team coordinate with the Commanding Officer of the Marine Corps Air Station at Cherry Point (252-466-2811) prior to project construction.

Traffic Noise (NCDOT Division 2, Design-Build team)

- Once the Traffic Noise Report is approved, the areas where noise abatement preliminarily meets feasibility and reasonableness criteria will be reported to the public by way of the U-5713 Project website (www.ncdot.gov/projects/US70_JamesCity/) and will also be mentioned in an upcoming project newsletter that will be sent prior to the final round of public meetings later in 2018. Additionally, hard copies of the TNR will be available for review during these public meetings and Traffic Noise experts will be in attendance to address any questions or concerns from the public.

Northern long-eared bat (NCDOT Division 2, Design-Build Team)

- Northern long-eared bat (NLEB) (*Myotis septentrionalis*)
The USFWS has developed a programmatic biological opinion (PBO) in conjunction with the Federal Highway Administration (FHWA), USACE, and NCDOT for NLEB in eastern North Carolina. The PBO covers the entire NCDOT program in Divisions 1-8, including all NCDOT projects and activities. The programmatic determination for NLEB is “**May Affect, Likely to Adversely Affect**”. The PBO provides incidental take coverage for NLEB and will ensure compliance with Section 7 of the Endangered Species Act for five years for all NCDOT projects with a federal nexus in Divisions 1-8, which includes Craven County, where TIP U-5713 and R-5777A&B are located. This level of incidental take is authorized from the effective date of a final listing determination through April 30, 2020.

I. Categorical Exclusion Approval

STIP Project No.	U-5713 and R-5777A&B
WBS Element	50111.1.FS1, 44648.1.2 & 44648.1.3
Federal Project No.	*NHPP-0070(182) [*U-5713 portion only]

Prepared By:

7/2/2018

Date

DocuSigned by:

Craig Young

DD3A49A9CC86481

Craig M. Young, PE; Senior Project Manager
Three Oaks Engineering, Inc.

Prepared For:

United States Department of Transportation
Federal Highway Administration
and
North Carolina Department of Transportation
Division of Highways

NCDOT certifies that the proposed action qualifies as a Type III Categorical Exclusion.

7/2/2018

Date

DocuSigned by:

John Conforti

5A29A6045D6B43C

John G. Conforti, REM; Senior Project Manager
North Carolina Department of Transportation – Technical Services Unit

FHWA Approval:

7/2/2018

Date

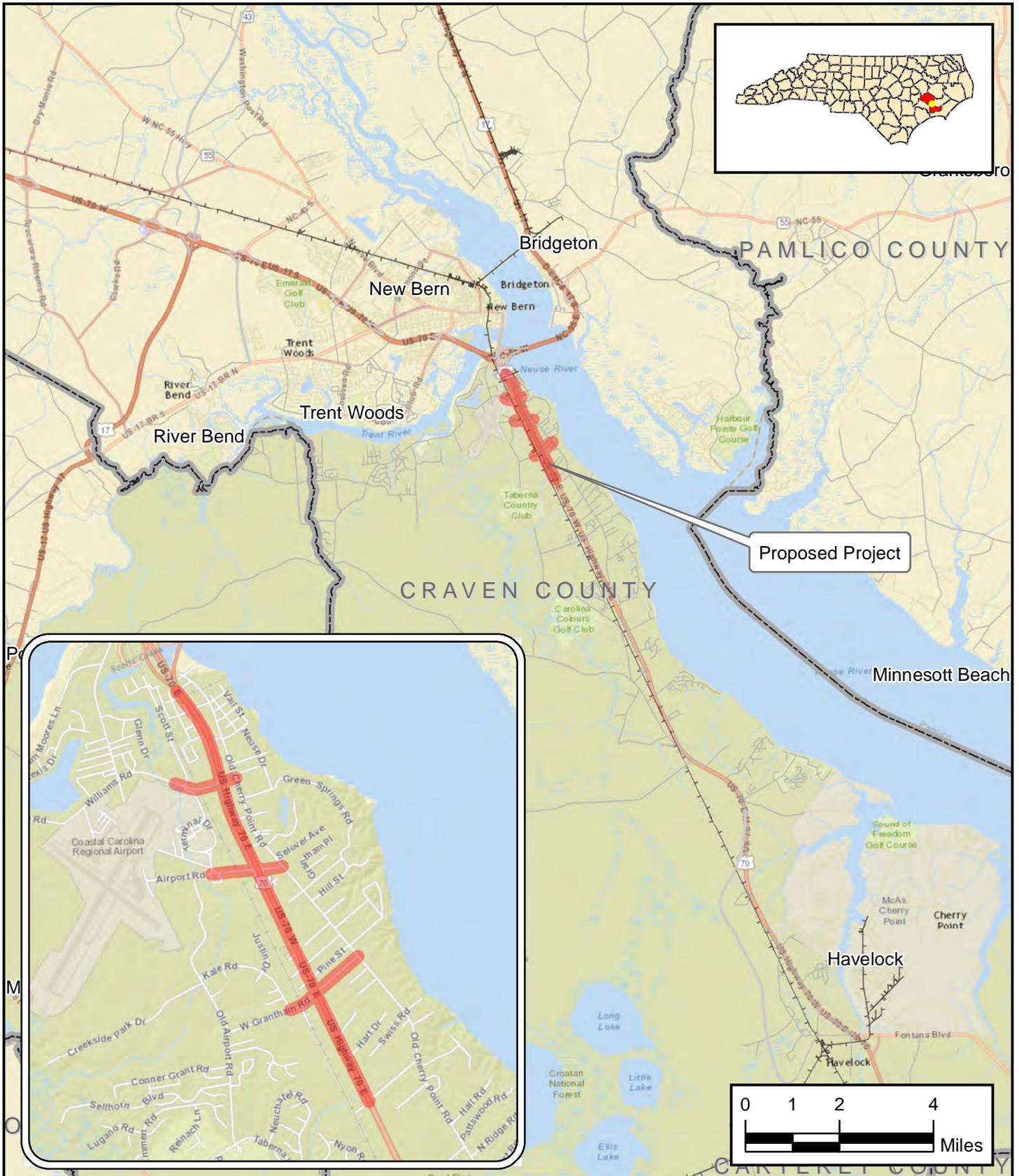
DocuSigned by:

Kristina Solberg

DB2E3AA00313429

John F. Sullivan, III, PE, Division Administrator
Federal Highway Administration

FIGURES



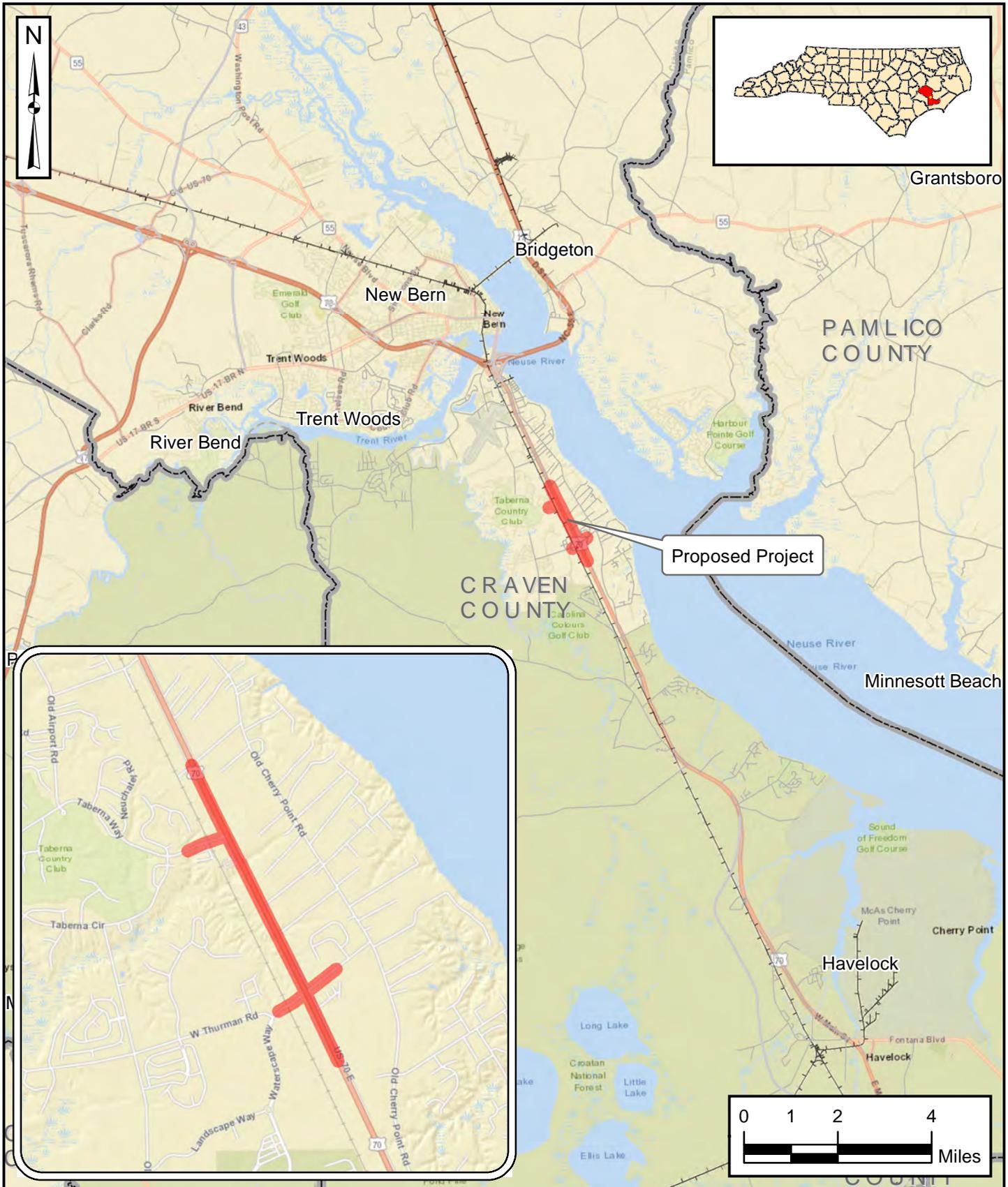
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 PROJECT DEVELOPMENT AND ENVIRONMENTAL ANALYSIS UNIT

**VICINITY MAP
 IMPROVEMENTS TO US 70
 FROM SR 1121 (GARNER RD)
 TO NEUSE RIVER BRIDGE**
 JAMES CITY, CRAVEN COUNTY
 TIP PROJECT U-5713



County:	CRAVEN
Div: 2	TIP# U-5713
WBS:	50111.1.FS1
Date:	APRIL 2015

**Figure
 1**

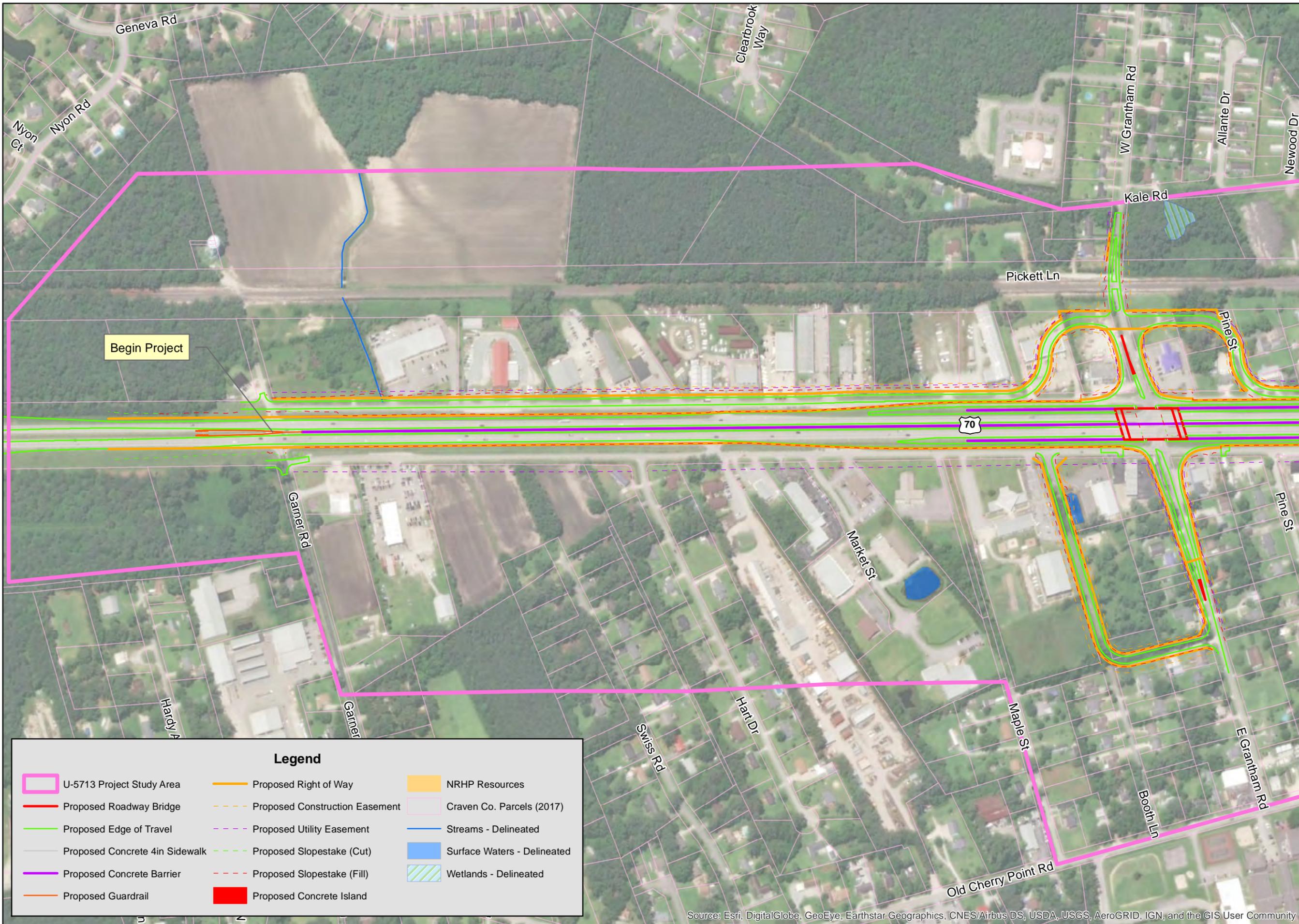


NORTH CAROLINA DEPARTMENT
OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROJECT DEVELOPMENT AND
ENVIRONMENTAL ANALYSIS UNIT

VICINITY MAP
US 70 INTERCHANGES
TABERNA WAY AND
E. THURMAN RD
JAMES CITY, CRAVEN COUNTY
STIP PROJECT R-5777 A&B

County:	CRAVEN
Div. 2	STIP# R-5777 A&B
WBS: 44648.1.2 / 44648.1.3	
Date:	SEPT 2017

Figure
1



Legend			
	U-5713 Project Study Area		NRHP Resources
	Proposed Roadway Bridge		Proposed Construction Easement
	Proposed Edge of Travel		Proposed Utility Easement
	Proposed Concrete 4in Sidewalk		Proposed Slopestake (Cut)
	Proposed Concrete Barrier		Proposed Slopestake (Fill)
	Proposed Guardrail		Proposed Concrete Island
			Craven Co. Parcels (2017)
			Streams - Delineated
			Surface Waters - Delineated
			Wetlands - Delineated



NORTH CAROLINA
DEPARTMENT
OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROJECT DESIGN MAP
US 70 IMPROVEMENTS
FROM SR 1124 (GARNER RD)
TO THE NEUSE RIVER BRIDGE
CRAVEN COUNTY
STIP PROJECT U-5713
WBS 50111.1.FS1



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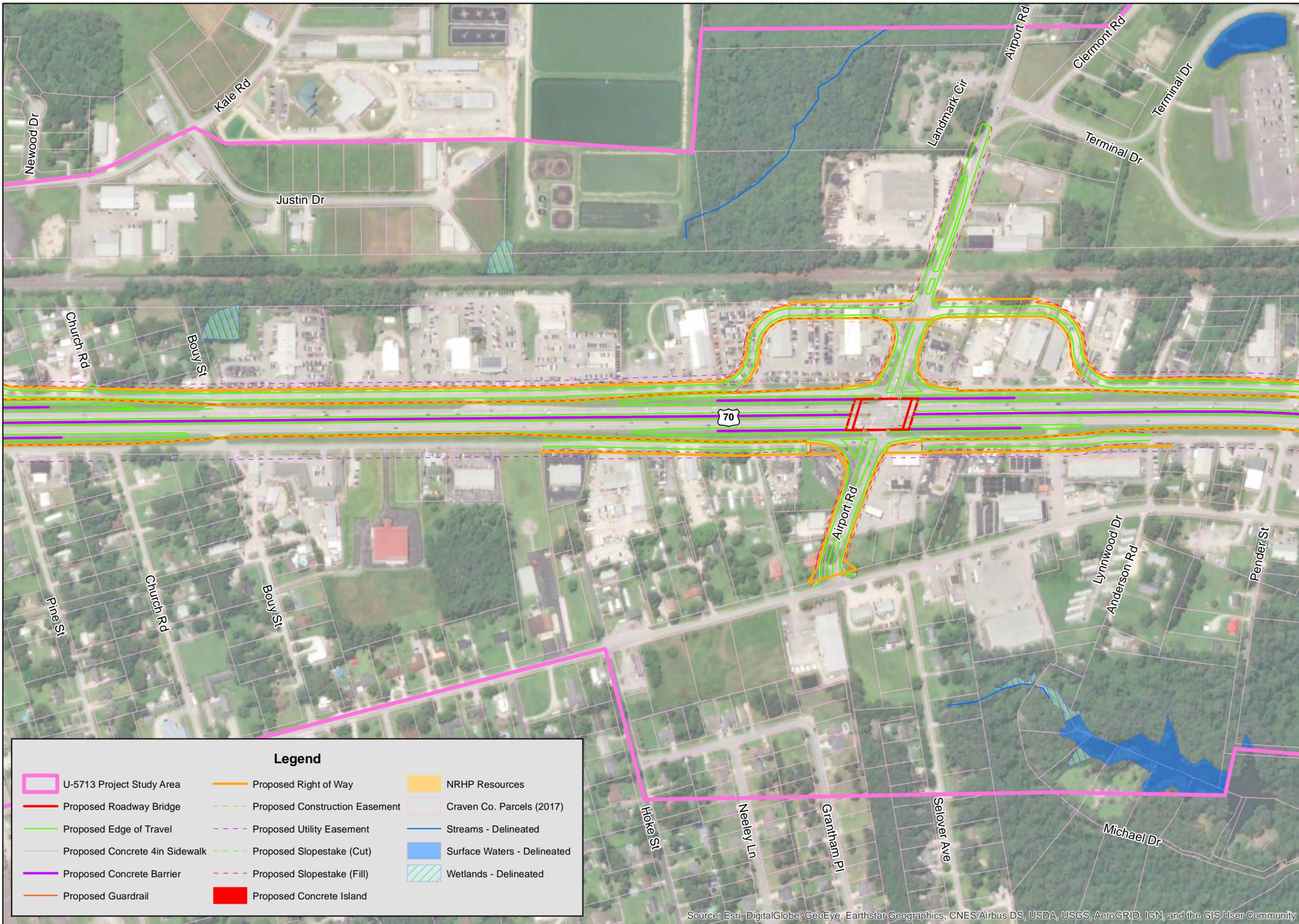
By:
C ROWELLS

County:
CRAVEN

Div: 2	STIP# U-5713
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Date:
JUNE 2018

Figure
2
Sheet 1 of 3



Legend			
	U-5713 Project Study Area		NRHP Resources
	Proposed Roadway Bridge		Craven Co. Parcels (2017)
	Proposed Edge of Travel		Streams - Delineated
	Proposed Concrete 4in Sidewalk		Surface Waters - Delineated
	Proposed Concrete Barrier		Wetlands - Delineated
	Proposed Guardrail		Proposed Concrete Island
	Proposed Right of Way		
	Proposed Construction Easement		
	Proposed Utility Easement		
	Proposed Slopestake (Cut)		
	Proposed Slopestake (Fill)		



NORTH CAROLINA
DEPARTMENT
OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROJECT DESIGN MAP
US 70 IMPROVEMENTS
FROM SR 1124 (GARNER RD)
TO THE NEUSE RIVER BRIDGE
CRAVEN COUNTY
STIP PROJECT U-5713
WBS 50111.1.FS1



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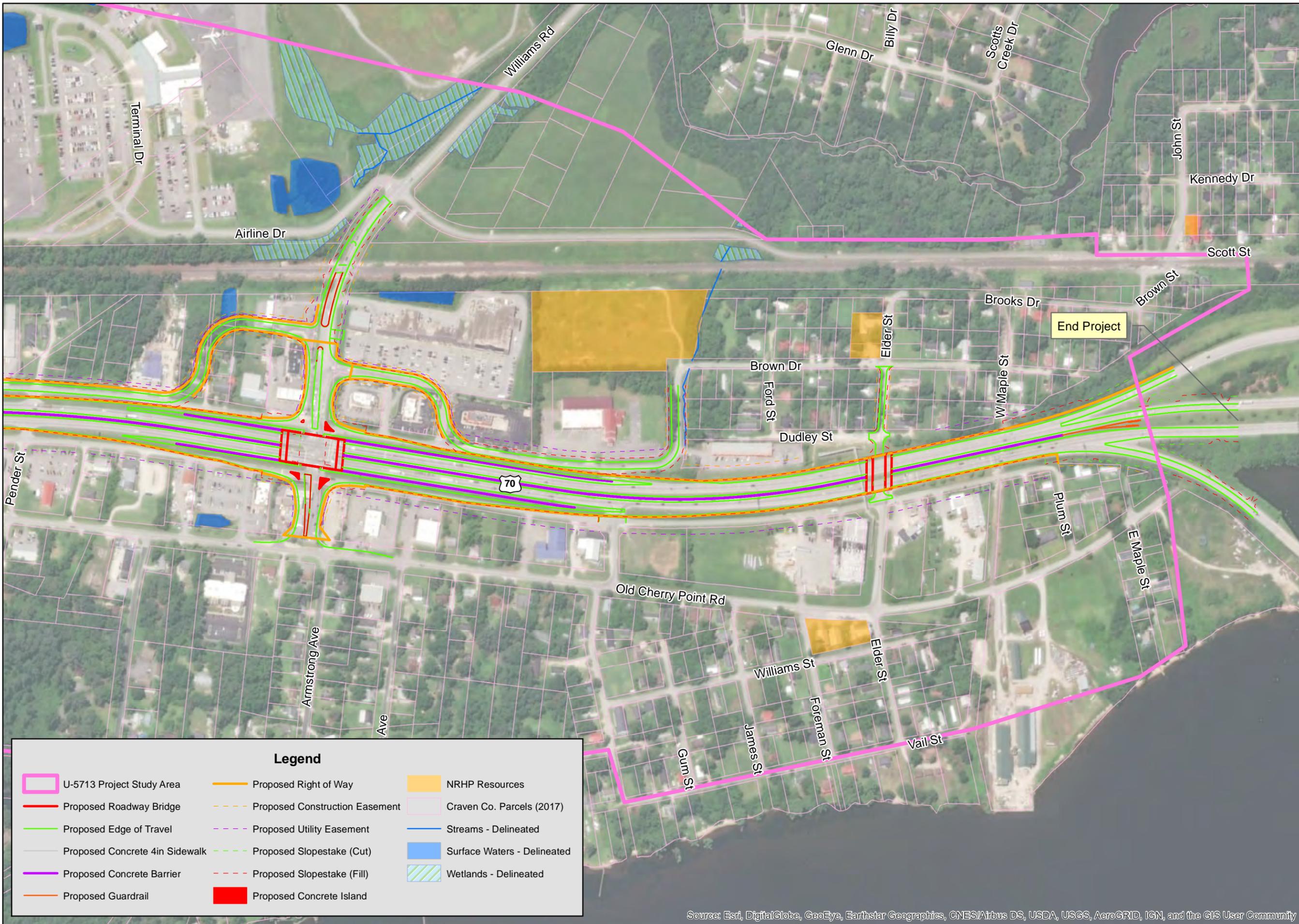
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C ROWELLS

County:
CRAVEN

Div: 2	STIP# U-5713
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Date:
JUNE 2018

Figure
2
Sheet 2 of 3



Legend					
	U-5713 Project Study Area		Proposed Right of Way		NRHP Resources
	Proposed Roadway Bridge		Proposed Construction Easement		Craven Co. Parcels (2017)
	Proposed Edge of Travel		Proposed Utility Easement		Streams - Delineated
	Proposed Concrete 4in Sidewalk		Proposed Slopestake (Cut)		Surface Waters - Delineated
	Proposed Concrete Barrier		Proposed Slopestake (Fill)		Wetlands - Delineated
	Proposed Guardrail		Proposed Concrete Island		



NORTH CAROLINA
DEPARTMENT
OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROJECT DESIGN MAP
US 70 IMPROVEMENTS
FROM SR 1124 (GARNER RD)
TO THE NEUSE RIVER BRIDGE
CRAVEN COUNTY
STIP PROJECT U-5713
WBS 50111.1.FS1



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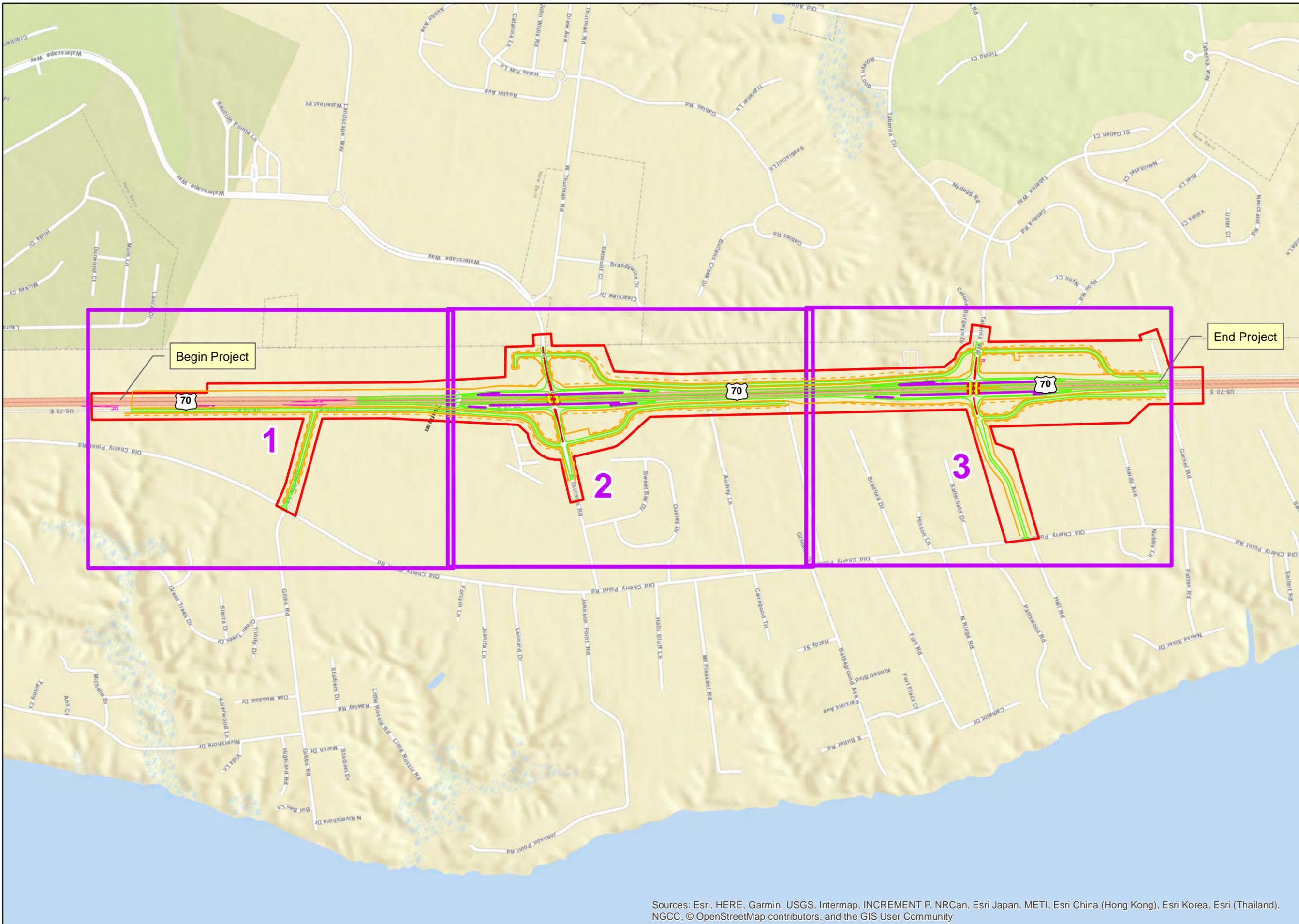
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County:
CRAVEN

Div: 2	STIP# U-5713
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Date:
JUNE 2018

Figure
2
Sheet 3 of 3



NORTH CAROLINA
DEPARTMENT
OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROJECT DESIGN MAP
US 70 IMPROVEMENTS
FROM APPROX. 1 MILE EAST
OF SR 1116 (E. THURMAN RD)
TO SR 1124 (GARNER RD)
CRAVEN COUNTY
STIP PROJECT R-5777A&B
WBS 44648.1.2 / 44648.1.3



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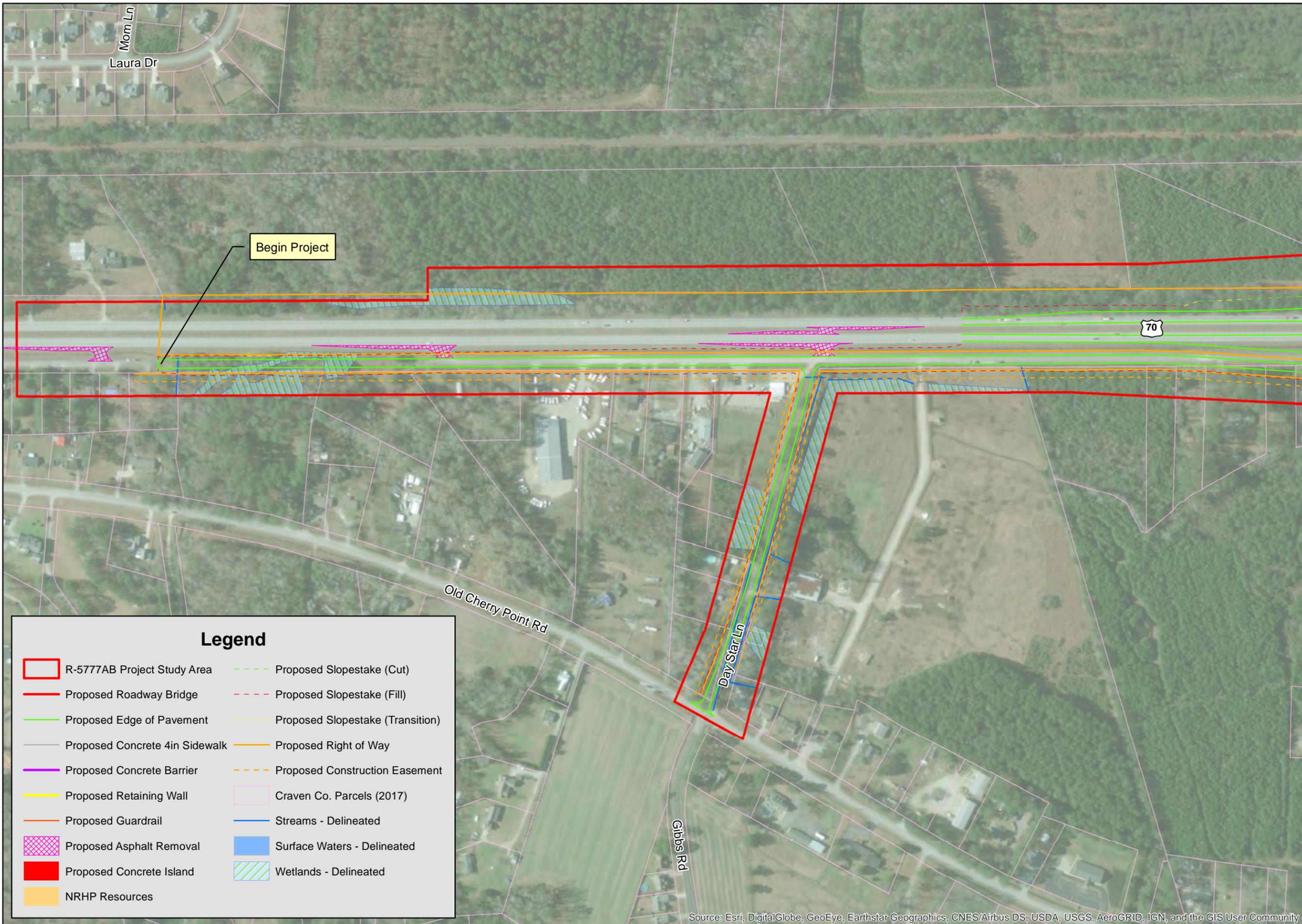
County:
CRAVEN

Div: 2	STIP# R-5777A&B
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Date:
JUNE 2018

Figure
2
Sheet Index

Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, © OpenStreetMap contributors, and the GIS User Community

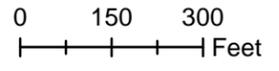


Legend	
	R-5777AB Project Study Area
	Proposed Roadway Bridge
	Proposed Edge of Pavement
	Proposed Concrete 4in Sidewalk
	Proposed Concrete Barrier
	Proposed Retaining Wall
	Proposed Guardrail
	Proposed Asphalt Removal
	Proposed Concrete Island
	NRHP Resources
	Proposed Slopestake (Cut)
	Proposed Slopestake (Fill)
	Proposed Slopestake (Transition)
	Proposed Right of Way
	Proposed Construction Easement
	Craven Co. Parcels (2017)
	Streams - Delineated
	Surface Waters - Delineated
	Wetlands - Delineated



NORTH CAROLINA
DEPARTMENT
OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROJECT DESIGN MAP
US 70 IMPROVEMENTS
FROM APPROX. 1 MILE EAST
OF SR 1116 (E. THURMAN RD)
TO SR 1124 (GARNER RD)
CRAVEN COUNTY
STIP PROJECT R-5777A&B
WBS 44648.1.2 / 44648.1.3



By:
C ROWELLS

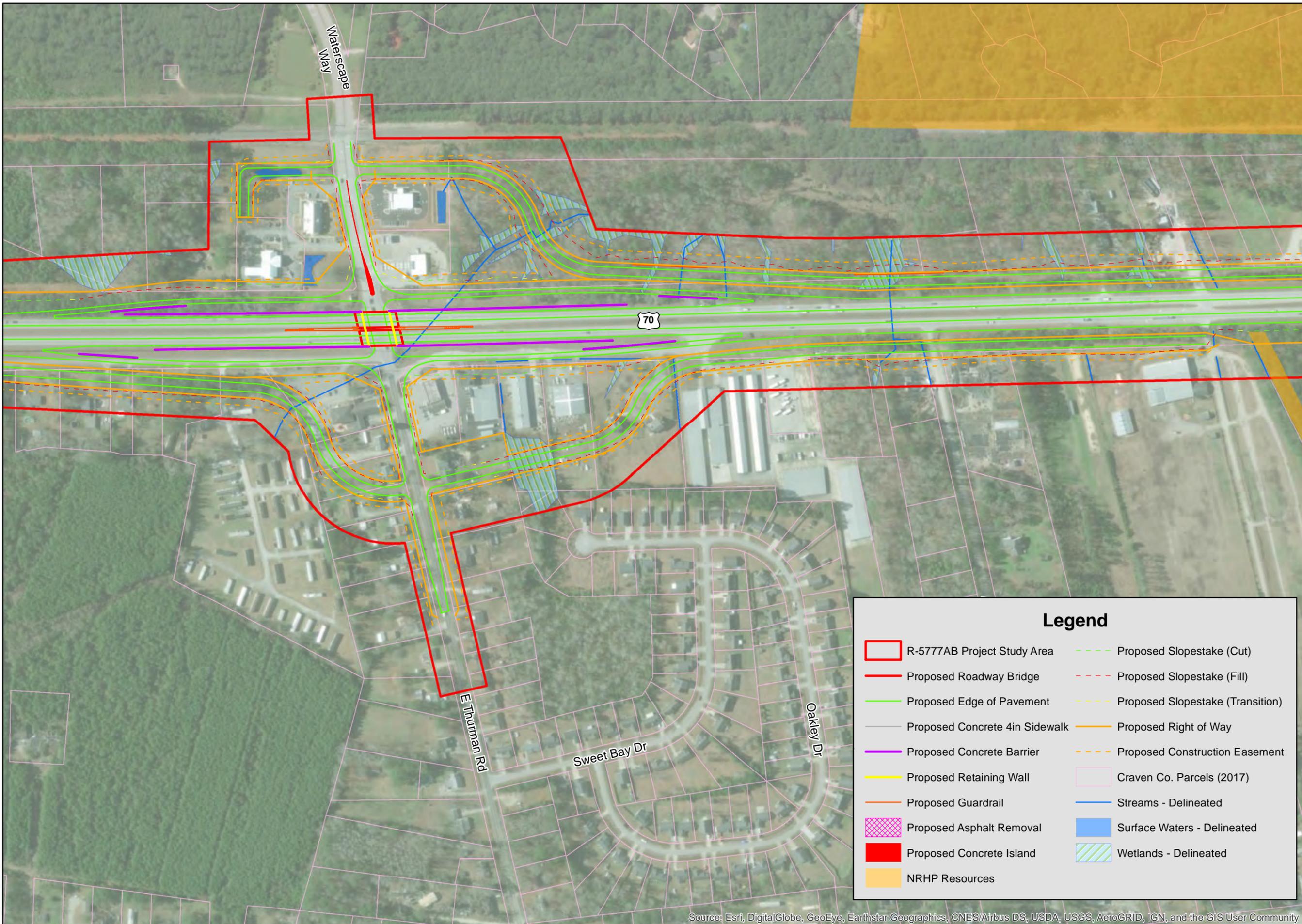
County:
CRAVEN

Div: 2	STIP# R-5777A&B
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Date:
JUNE 2018

Figure
2
Sheet 1 of 3

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



Legend

R-5777AB Project Study Area	Proposed Slopestake (Cut)
Proposed Roadway Bridge	Proposed Slopestake (Fill)
Proposed Edge of Pavement	Proposed Slopestake (Transition)
Proposed Concrete 4in Sidewalk	Proposed Right of Way
Proposed Concrete Barrier	Proposed Construction Easement
Proposed Retaining Wall	Craven Co. Parcels (2017)
Proposed Guardrail	Streams - Delineated
Proposed Asphalt Removal	Surface Waters - Delineated
Proposed Concrete Island	Wetlands - Delineated
NRHP Resources	

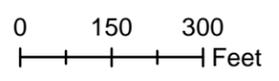
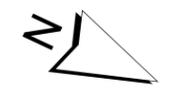
Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



NORTH CAROLINA
DEPARTMENT
OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROJECT DESIGN MAP
US 70 IMPROVEMENTS
FROM APPROX. 1 MILE EAST
OF SR 1116 (E. THURMAN RD)
TO SR 1124 (GARNER RD)

CRAVEN COUNTY
STIP PROJECT R-5777A&B
WBS 44648.1.2 / 44648.1.3



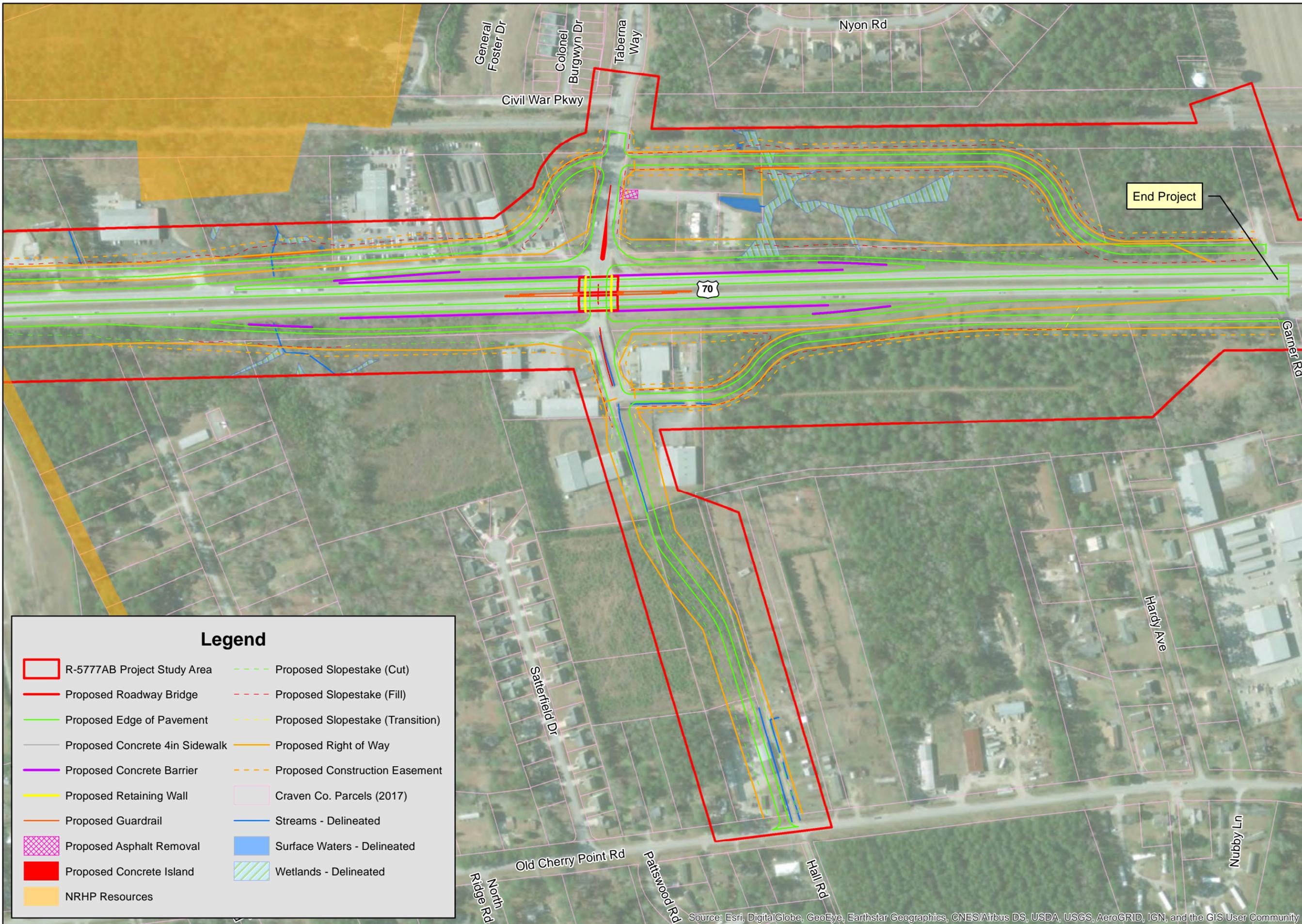
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County:
CRAVEN

Div: 2	STIP# R-5777A&B
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Date:
JUNE 2018

Figure
2
Sheet 2 of 3



Legend

R-5777AB Project Study Area	Proposed Slopestake (Cut)
Proposed Roadway Bridge	Proposed Slopestake (Fill)
Proposed Edge of Pavement	Proposed Slopestake (Transition)
Proposed Concrete 4in Sidewalk	Proposed Right of Way
Proposed Concrete Barrier	Proposed Construction Easement
Proposed Retaining Wall	Craven Co. Parcels (2017)
Proposed Guardrail	Streams - Delineated
Proposed Asphalt Removal	Surface Waters - Delineated
Proposed Concrete Island	Wetlands - Delineated
NRHP Resources	



NORTH CAROLINA
DEPARTMENT
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PROJECT DESIGN MAP
US 70 IMPROVEMENTS
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OF SR 1116 (E. THURMAN RD)
TO SR 1124 (GARNER RD)
CRAVEN COUNTY
STIP PROJECT R-5777A&B
WBS 44648.1.2 / 44648.1.3



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By:
C ROWELLS

County:
CRAVEN

Div: 2	STIP# R-5777A&B
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Date:
JUNE 2018

Figure
2
Sheet 3 of 3

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

APPENDIX



HISTORIC ARCHITECTURE AND LANDSCAPES ASSESSMENT OF EFFECTS FORM

This form only pertains to Historic Architecture and Landscapes for this project. It is not valid for Archaeological Resources. You must consult separately with the Archaeology Group.

PROJECT INFORMATION

Project No:	U-5713	County:	Craven
WBS No.:	50111.1.1	Document Type:	
Fed. Aid No:	NHPP-0070(182)	Funding:	State <input checked="" type="checkbox"/> Federal
Federal Permit(s):	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	Permit Type(s):	Not specified in review request
Project Description: Improvements to US 70 from SR 1121 (Garner Road) to the Neuse River Bridge in James City (no off-site detour specified in review request).			

SUMMARY OF HISTORIC ARCHITECTURE AND LANDSCAPES REVIEW

<p><u>DESCRIPTION OF REVIEW ACTIVITIES, RESULTS, AND CONCLUSIONS:</u> ARCHITECTURAL REVIEW OF U-5713 BEGAN AS A STANDARD TIP PROJECT, AND CAME TO BE REDEFINED UNDER THE PA. IN MARCH 2016 NCDOT – HISTORIC ARCHITECTURE ENGAGED NEW SOUTH ASSOCIATES TO CARRY OUT A COMPREHENSIVE SURVEY OF THE STUDY AREA. OF THE 222 RESOURCES RECORDED, 13 WARRANTED FURTHER INVESTIGATION (ELIGIBILITY REVIEW JULY 2016 BY HPO AND NCDOT). IN JULY 2016 NEW SOUTH COMMENCED RESEARCH AND EVALUATION AND PRESENTED FINDINGS IN A JANUARY 2017 TECHNICAL REPORT (AVAILABLE ON CONNECT NCDOT (HTTPS://CONNECT.NCDOT.GOV)). IN MARCH OF 2017, HPO CONCURRED THAT THE PILGRIM CHAPEL MISSIONARY BAPTIST CHURCH (CV0396), THE JONES CHAPEL AME ZION CHURCH (CV1902), AND THE MEADOWS CEMETERY (CV2783) ARE ELIGIBLE FOR LISTING IN THE NATIONAL REGISTER AND THE MT. SHILOH MISSIONARY BAPTIST CHURCH (CV2050) RETAINS THE ELEMENTS FOR WHICH IT WAS NR-LISTED. HPO ALSO AGREED TO THE ESTABLISHED AND PROPOSED NR BOUNDARIES FOR EACH PROPERTY. AN EFFECTS CONSULTATION IS REQUIRED TO ADDRESS HOW THE PROJECT MAY AFFECT EACH OF THE FOUR PROPERTIES.</p>

ASSESSMENT OF EFFECTS

Property Name:	Mt. Shiloh Missionary Baptist Church	Status:	NR
Survey Site No.:	CV2050	PIN:	7-005-0613
Effects			
<input checked="" type="checkbox"/> No Effect <input type="checkbox"/> No Adverse Effect <input type="checkbox"/> Adverse Effect			
<u>Explanation of Effects Determination:</u>			
<p>Proposed construction activities will neither physically, nor visually impact the property. Church is located just outside the northern end of the APE; several streets and multiple properties intervene between the church and US 70.</p>			

<u>List of Environmental Commitments:</u> -

FHWA Intends to use the State Historic Preservation Office's concurrence as a basis for a "de minimis" finding for the following properties, pursuant to Section 4(f):

ASSESSMENT OF EFFECTS

Property Name:	Pilgrim Chapel Missionary Baptist Church	Status:	DE
Survey Site No.:	CV0396	PIN:	7-006-129
Effects			
<input checked="" type="checkbox"/> No Effect <input type="checkbox"/> No Adverse Effect <input type="checkbox"/> Adverse Effect			
<u>Explanation of Effects Determination:</u>			
Proposed construction activities, including demolition of pedestrian bridge over US 70 and the Elder Street crossing improvements, will neither physically, nor visually impact the property.			
<u>List of Environmental Commitments:</u> -			

FHWA Intends to use the State Historic Preservation Office's concurrence as a basis for a "de minimis" finding for the following properties, pursuant to Section 4(f):

ASSESSMENT OF EFFECTS

Property Name:	Jones Chapel AME Zion Church	Status:	DE
Survey Site No.:	CV1902	PIN:	7-006-019
Effects			
<input checked="" type="checkbox"/> No Effect <input type="checkbox"/> No Adverse Effect <input type="checkbox"/> Adverse Effect			
<u>Explanation of Effects Determination:</u>			
Proposed construction activities, including demolition of pedestrian bridge over US 70 and the Elder Street crossing improvements, will neither physically, nor visually impact the property. Improvements to Elder street to be confined to the block east of the church, between Brown Drive and US 70.			

List of Environmental Commitments: -

FHWA Intends to use the State Historic Preservation Office's concurrence as a basis for a "de minimis" finding for the following properties, pursuant to Section 4(f):

ASSESSMENT OF EFFECTS

<i>Property Name:</i>	Meadows Cemetery	<i>Status:</i>	DE
<i>Survey Site No.:</i>	CV2783	<i>PIN:</i>	7-009-102
<i>Effects</i> X No Effect <input type="checkbox"/> No Adverse Effect <input type="checkbox"/> Adverse Effect			
<u><i>Explanation of Effects Determination:</i></u> Proposed construction activities, including creation of a service road to the east of the cemetery, will neither physically, nor visually impact the property. Brown Drive improvements will not alter or impede access to the cemetery entrance.			
<u><i>List of Environmental Commitments:</i></u> -			

FHWA Intends to use the State Historic Preservation Office's concurrence as a basis for a "de minimis" finding for the following properties, pursuant to Section 4(f):

**Technical report and photographs on file
NCDOT- Historic Architecture and NCHPO**



North Carolina Department of Cultural Resources
State Historic Preservation Office

Ramona M. Bartos, Administrator

Governor Pat McCrory
Secretary Susan Kluttz

Office of Archives and History
Deputy Secretary Kevin Cherry

May 28, 2015

MEMORANDUM

TO: Richard W. Hancock, P.E., Manager
Project Development and Environmental Analysis Unit
NCDOT Division of Highways

FROM: Ramona M. Bartos *RMB for Ramona M. Bartos*

SUBJECT: Start of Study for Improvements to US 70 from SR 1121 to Neuse River Bridge, U-5713,
Craven County, ER 15-0988

Thank you for your letter of April 20, 2015, start of study letter for the above-referenced project.

There are no known archaeological sites within the proposed project area. Based on our knowledge of the area, it is unlikely that any archaeological resources that may be eligible for inclusion in the National Register of Historic Places will be affected by the project. We, therefore, recommend that no archaeological investigation be conducted in connection with this project.

As there has been no architectural survey work in the study area since 2001, we recommend that NCDOT undertake an architectural survey of any properties over fifty years of age in the Area of Potential Effects.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579 or environmental.review@ncdot.gov. In all future communication concerning this project, please cite the above referenced tracking number.

cc: Mary Pope Furr, NCDOT, mfurr@ncdot.gov
Matt Wilkerson, NCDOT, mtwilkerson@ncdot.gov

17-09-0015

17-09-0016



HISTORIC ARCHITECTURE AND LANDSCAPES ASSESSMENT OF EFFECTS FORM

Supersedes form dated 20 March 2018

This form only pertains to Historic Architecture and Landscapes for this project. It is not valid for Archaeological Resources. You must consult separately with the Archaeology Group.

PROJECT INFORMATION

Project No:	R-5777A&B	County:	Craven
WBS No.:	44648.1.1	Document Type:	
Fed. Aid No:		Funding:	X State <input type="checkbox"/> Federal
Federal Permit(s):	X Yes <input type="checkbox"/> No	Permit Type(s):	Not specified in review request
Project Description: Convert SR 1922 (Taberna Way) and SR 1116 (West and East Thurman Road)/US 70 at-grade intersections to interchanges (no off-site detour specified in review request).			

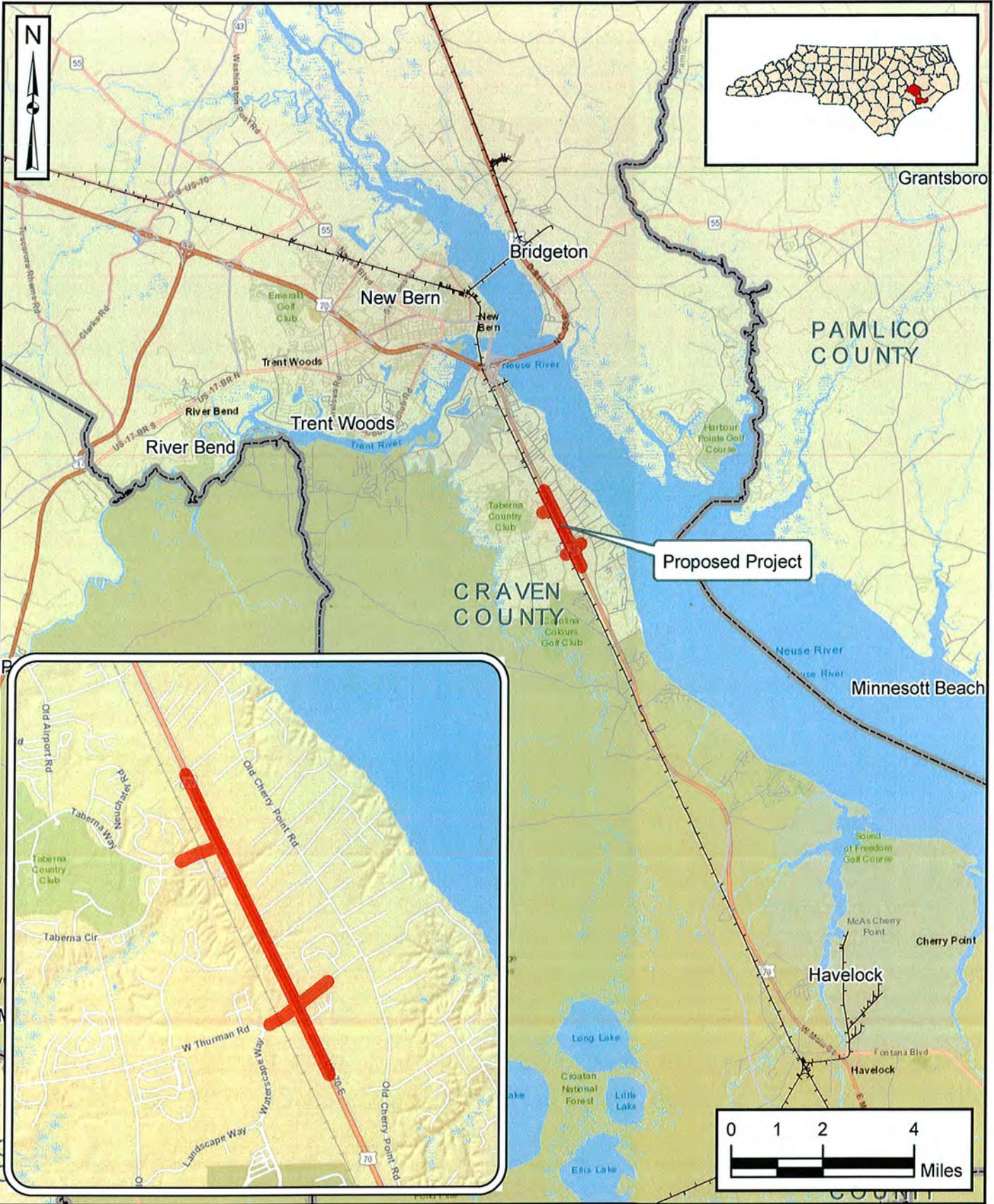
SUMMARY OF HISTORIC ARCHITECTURE AND LANDSCAPES REVIEW

DESCRIPTION OF REVIEW ACTIVITIES, RESULTS, AND CONCLUSIONS: HPOWeb reviewed on 17 October 2017 and yielded one NR, two SS, and no DE, SL, or LD properties in the Area of Potential Effects (APE). APE equates with the study area provided in the review request (see attached map). Northern end of study area partially overlies that of the U-5713 project. Craven County current GIS mapping, aerial photography, and tax information indicated a mostly developed APE containing about 215 built resources, of which about 79% are residential in use and about 34% predate 1970 (viewed 17 October 2017). The pre-1970s resources are unexceptional (many altered) examples of their types. The previously recorded Service Station (CV2782) and House (CV2716) were determined ineligible for the National Register by the 2017 architectural study for the U-5713 project, a finding that remains valid. Part of the National Register-listed New Bern Battlefield Site (CV2055) lies within the study area (also within a possible permit area), and thus impacts of the project on the resource must be considered and discussed with the state historic preservation office and United States Army Corps of Engineers (see attached aerial maps).

Google Maps "Street View" and other visuals confirmed the presence and relative placement of the historic architectural and landscape resources in the APE (viewed 17 October 2017). The comprehensive county architectural survey (1980-3) recorded no properties in the APE, but did reference the Battle of New Bern (Peter B. Sandbeck, *The Historic Architecture of New Bern and Craven County, North Carolina* (New Bern: Tryon Palace Commission, 1988)). Of later studies, only that for the U-5713 project identified any resources in the APE as noted above. The A and B sections of the R-5777 project generated separate review requests, and this form is filed with each and pertains to both. This form supersedes that dated 20 March 2018 and documents the finding established at a 15 May 2018 effects consultation necessitated by the revised boundary for the New Bern Battlefield Site (CV2055).

ASSESSMENT OF EFFECTS

Property Name:	New Bern Battlefield Site	Status:	NR
Survey Site No.:	CV2055	PIN:	Multiple
Effects			
<input type="checkbox"/> No Effect <input checked="" type="checkbox"/> No Adverse Effect <input type="checkbox"/> Adverse Effect			



NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 PROJECT DEVELOPMENT AND ENVIRONMENTAL ANALYSIS UNIT

VICINITY MAP
US 70 INTERCHANGES
TABERNA WAY AND
E. THURMAN RD
 JAMES CITY, CRAVEN COUNTY
 STIP PROJECT R-5777 A&B

County:	CRAVEN
Div. 2	STIP# R-5777 A&B
WBS:	44648.1.2 / 44648.1.3
Date:	SEPT 2017

Figure
1

Tracking Nos. 17-09-0015# -0016



By: C ROWELLS	County: CRAVEN	Div. 2	Date: SEPTEMBER 2017
		STIP# R-5777A&B	



ENVIRONMENTAL FEATURES MAP
US 70 INTERCHANGES
TABERNA WAY AND
E. THURMAN RD

CRAVEN COUNTY
 STIP PROJECT R-5777A&B
 WBS 44648.1.2 / 44648.1.3

NORTH CAROLINA DEPARTMENT
 OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 PROJECT DEVELOPMENT AND
 ENVIRONMENTAL ANALYSIS UNIT



2

Figure

Tracking Nos. 17-09-0015
 17-09-0016



National Register Boundary – New Bern Battlefield Site (CV2055)

Please note: property contains two discontinuous sections.

R-5777A&B, Craven County

Base map: HPOWeb, nts



R-5777A&B
WBS No. 44648.1.1

US 70 Intersection Improvements

Craven County
Design Detail, May 2018



ARCHAEOLOGICAL SURVEY REQUIRED FORM

This form only pertains to ARCHAEOLOGICAL RESOURCES for this project. It is not valid for Historic Architecture and Landscapes. You must consult separately with the Historic Architecture and Landscapes Group.



PROJECT INFORMATION

Project No: R-5777a *County:* Craven
WBS No: 44648.1.2 *Document:* EXCLUSION
Federal Aid No: *Funding:* State Federal
Federal Permit Required? Yes No *Permit Type:*

Project Description: NOTE: This is an update of the Archaeological Survey Required form submitted on 10/23/2017.

Convert at-grade intersection of US 70 and Taberna Way to a restricted interchange in Craven County. Area of Potential Effects (A.P.E.) is approximately 4,103 meters (13,461 ft.) long and between 100-273 meters (328-894 ft.) wide. The A.P.E. (for this project as well as R-5777B) stretches along US 70 from Garner Rd. south to a point approximately 1,719 meters (5,638 ft.) south of Thurman Rd. It encompasses possible interchanges at US 70 and Taberna Way (R-5777A) and US 70 and Thurman Rd. (R-5777B; see PA 17-09-0016). (This A.P.E. is longer and narrower than the October 2017 study area.) The project is State-funded, may require Federal permits, and will not require easements.

SUMMARY OF ARCHAEOLOGICAL RESOURCES REVIEW: *SURVEY REQUIRED*

Brief description of review activities, results of review, and conclusions:

The original review (October 2017) included an examination of a topographic map, the Craven County soil survey, an aerial photograph, and listings of previously recorded sites, previous archaeological surveys, and previous environmental reviews at the Office of State Archaeology (O.S.A.). US 70 is oriented approximately north-south.

The topographic map (New Bern, N.C.) shows the landforms in the A.P.E. are mostly upland flats. The A.P.E. does include the headwaters of several tributary streams of Brice Creek which is located to the west of US 70. The level, well-drained uplands adjacent to the tributaries have some potential for prehistoric archaeological sites. The uplands away from water have less potential. The October 2017 review identified five areas within the large study area that had potential for archaeological sites.

The current review of the Craven County soil survey shows there are eight soil types in the A.P.E. The majority of the acreage (approx. 60%) within the A.P.E. is poorly-drained soil. Four of the soil types are described as poorly-drained, while four are described as well-drained. The soil types within the A.P.E. are:

Craven silt loam (1-4% slopes)- moderately well-drained (13%)
Exum silt loam (0-2% slopes)- moderately well-drained (13%)
Goldsboro loamy fine sand (0-2% slopes)- moderately well-drained (7%)
Grantham silt loam- poorly-drained (13%)
Leaf silt loam- poorly-drained (37%)
Masontown mucky fine sandy loam, frequently-flooded- very poorly-drained (1%)
Rains fine sandy loam (0-2% slopes)- poorly-drained (11%)
Suffolk loamy sand (10-30% slopes)- well-drained (5%)

A review of information at the O.S.A. showed no previously recorded archaeological sites within or adjacent to the large study area. The study area had not been previously surveyed for archaeological sites. There are a number of projects (ER 83-1300; ER 93-7668; ER 02-11739; ER 05-1140; ER 08-1394; ER 13-1523; ER 13-2847) within or adjacent to the study area that have been reviewed by the State Historic Preservation Office (HPO). HPO did not recommend an archaeological survey for most of the projects. A survey was recommended for one project (ER 93-7668), a large development west of the A.P.E. (We could find no record that a survey had ever been conducted.)

One area near the A.P.E. along the west side of US 70 and the south side of Taberna Way has been surveyed. Russ (2002) surveyed the location of the proposed Battle of New Bern Park. The survey included the future locations of a visitor's center and parking lot, and did not identify any archaeological sites. Since the time of the survey the New Bern Battlefield Park has been established as an historical park (managed by the Craven County Historical Society). It is located outside of the A.P.E., on the west side of the railroad and south of Taberna Way.

Although not recorded as an archaeological site, the New Bern Battlefield Site is a National Register of Historic Places (NRHP)-listed site shown on the HPO web-based GIS site. The NRHP application (Brent 2001) describes it as a 98-acre site on the west side of the railroad tracks, and a narrow line of earthworks along the east side of US 70. It was listed on the NRHP in 2001. The battlefield site is located outside of the A.P.E. The southwest end of the earthworks is located within the A.P.E. The earthworks run southwest to northeast, a short distance south of and parallel with SR 1185 (Bradford Dr.). They run between US 70 at the southwest end and SR 1113 (Old Cherry Point Rd.) at the northeast end.

The original study area for this project was very large in order to accommodate various alternatives for this interchange (as well as for the proposed interchange at US 70 and Thurman Road to the south [see PA 17-09-0016; R-5777B]). There were several areas with archaeological potential, and these would need to be investigated if they fell within the final design. Also, the New Bern Battlefield site would be examined if the final design impacted any areas within its boundaries.

The smaller A.P.E. submitted in January 2018 will impact less of the high potential areas. Most of the A.P.E. is along each side of US 70, with expanded areas at the Taberna Way and Thurman Rd. interchanges. It is likely that at least some of the areas with archaeological potential are disturbed by roadside maintenance, drainage, utilities, etc. Examination of the aerial photograph shows the one at the north end of the A.P.E. is occupied by development. However, there are five small parts of the A.P.E. that are within the archaeological potential areas, are not visibly developed or disturbed, and have well-drained soils. One of these also includes the southwest

end of the earthworks. As mentioned above, the NRHP-listed New Bern Battlefield site is located outside of the A.P.E.

Recommend survey of the level, well-drained, undeveloped areas near water (as shown on the maps below). Determine whether the New Bern Battlefield earthworks are within the A.P.E. along the east side of US 70.

References Cited

Brent, Joseph E.

2001 National Register of Historic Places REgistration form for the "New Bern Battlefield Sites." Form prepared by Mudpuppy and Waterdog, Inc., Versailles, Kentucky on June 1, 2001.

Russ, Terri A.

2002 Phase I Cultural Resources Survey of the Proposed Battle of New Bern Park, Craven County, North Carolina. Report submitted to the New Bern Historical Society, New Bern, North Carolina. Phelps Archaeology Laboratory, East Carolina University, Greenville, North Carolina.

SUPPORT DOCUMENTATION

See attached: Map(s) Previous Survey Info Photos Correspondence
Other:

FINDING BY NCDOT ARCHAEOLOGIST – *SURVEY REQUIRED*

Caleb Smith

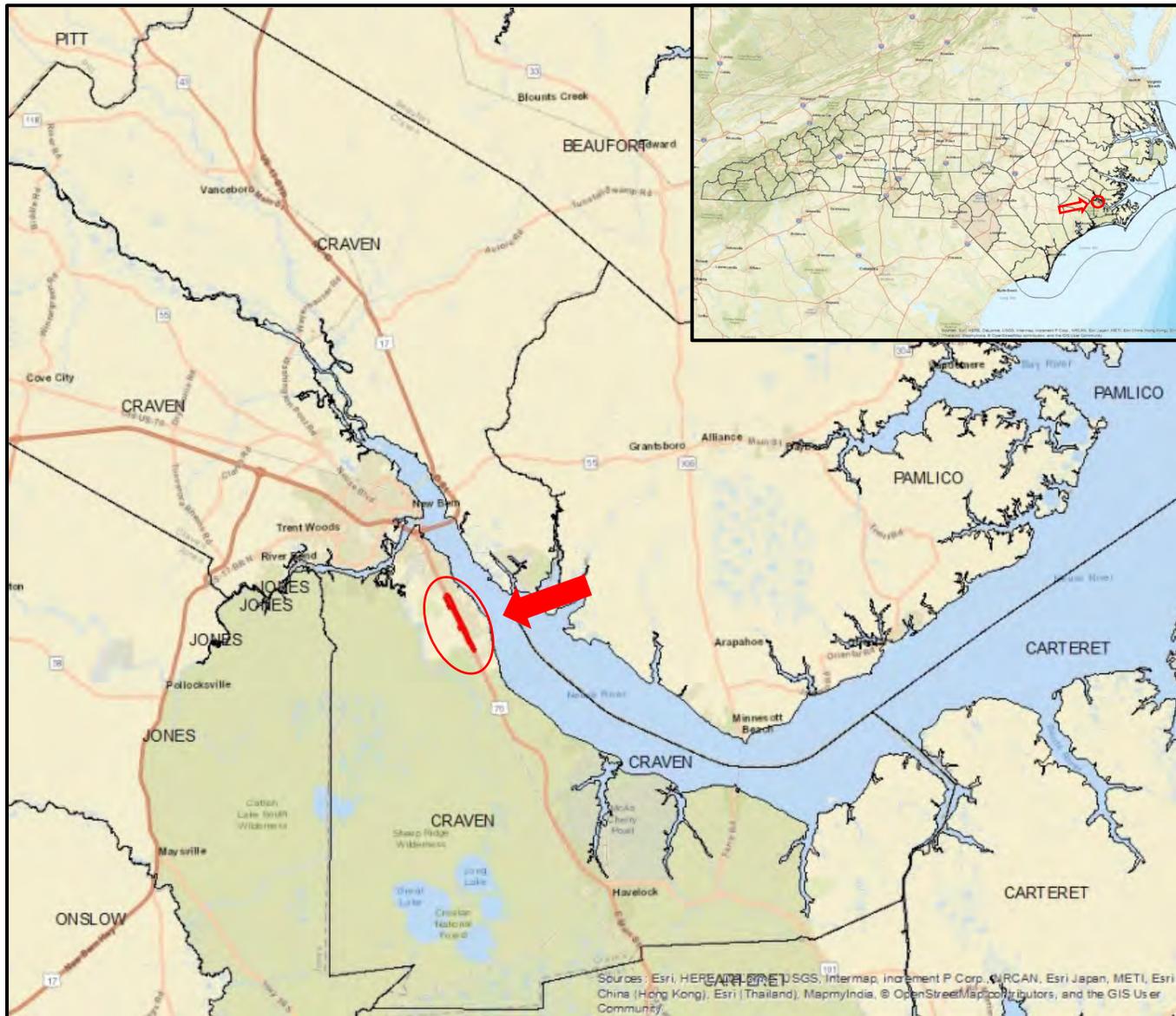
2/26/2018

NCDOT ARCHAEOLOGIST

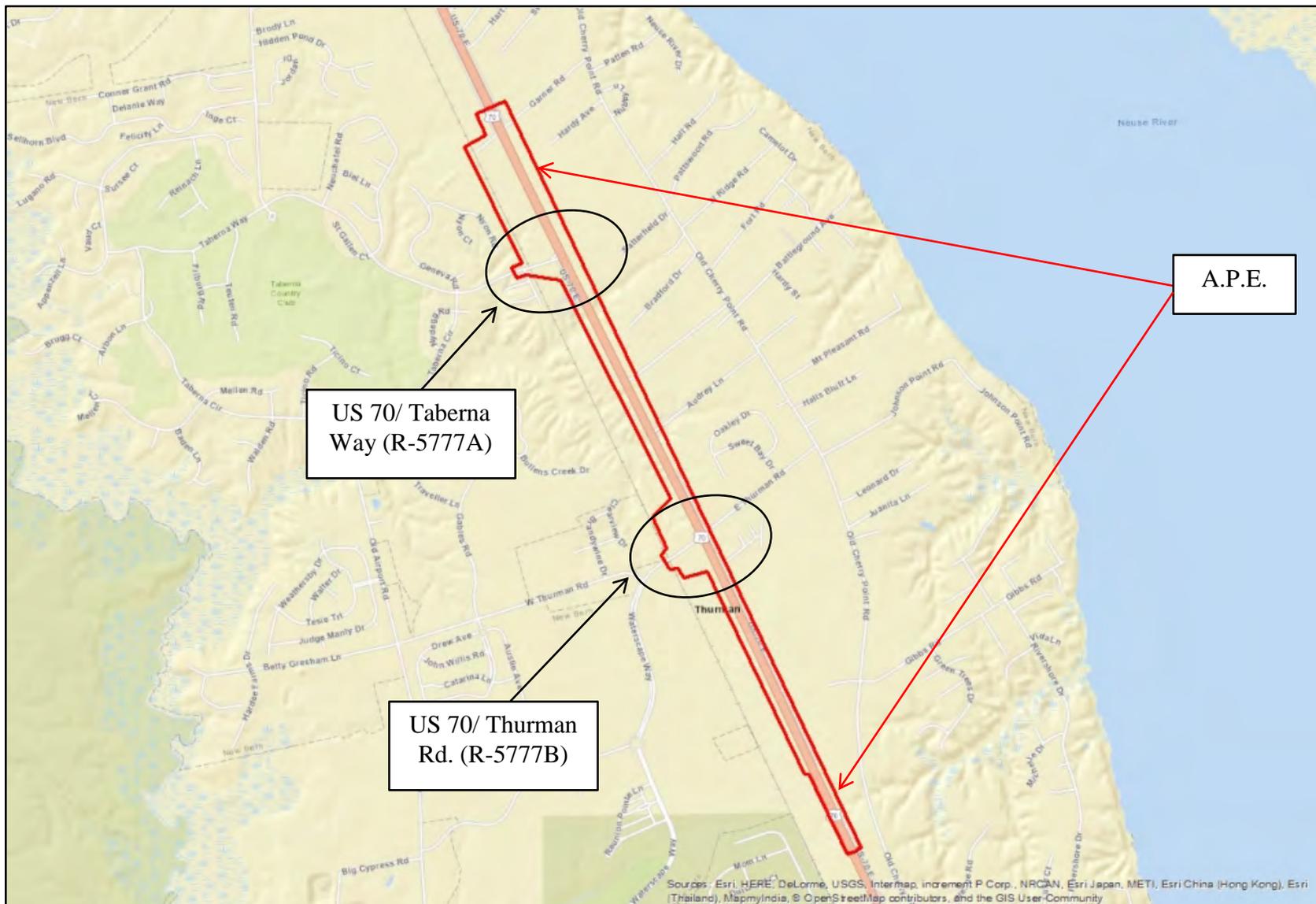
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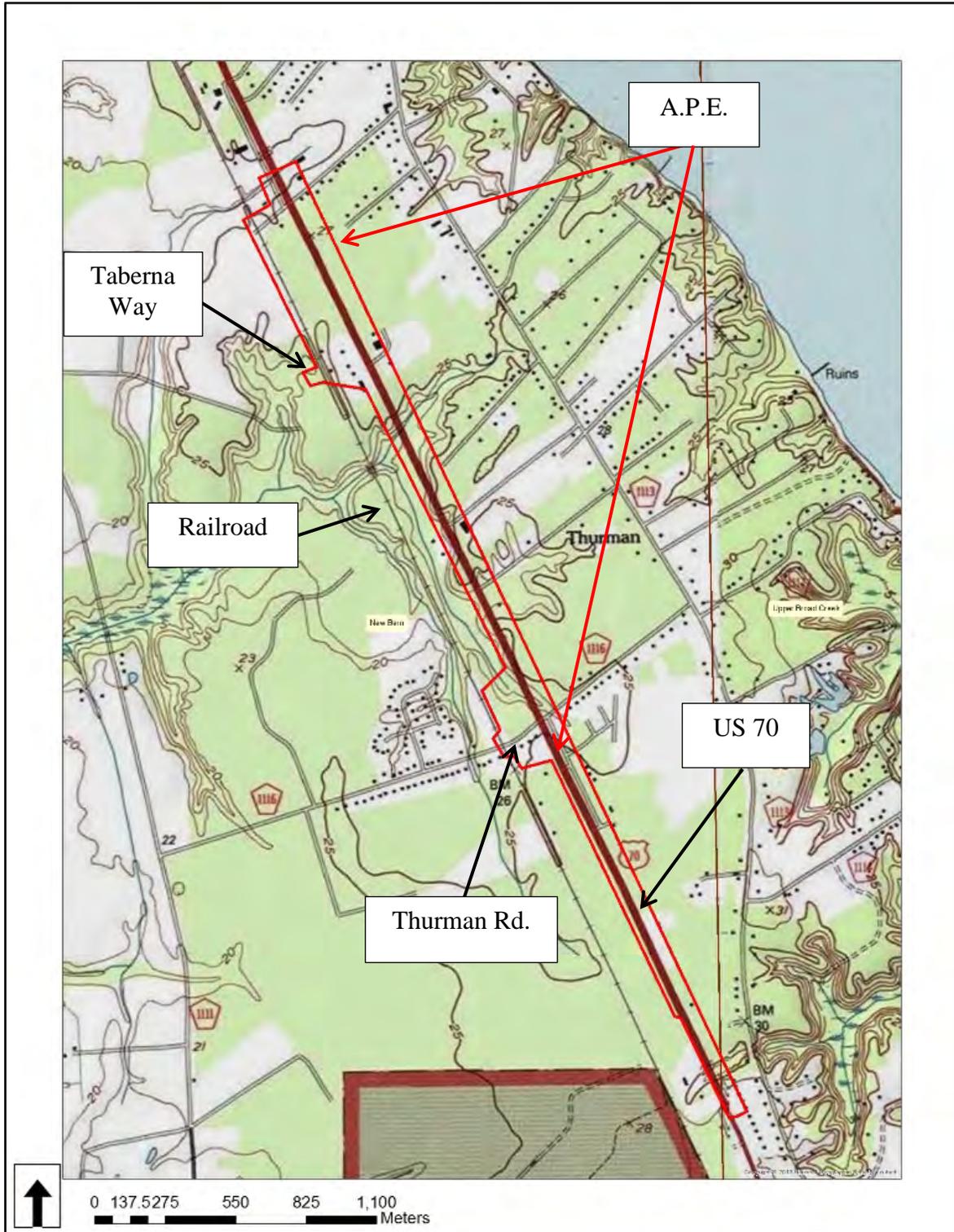
5/21/2018

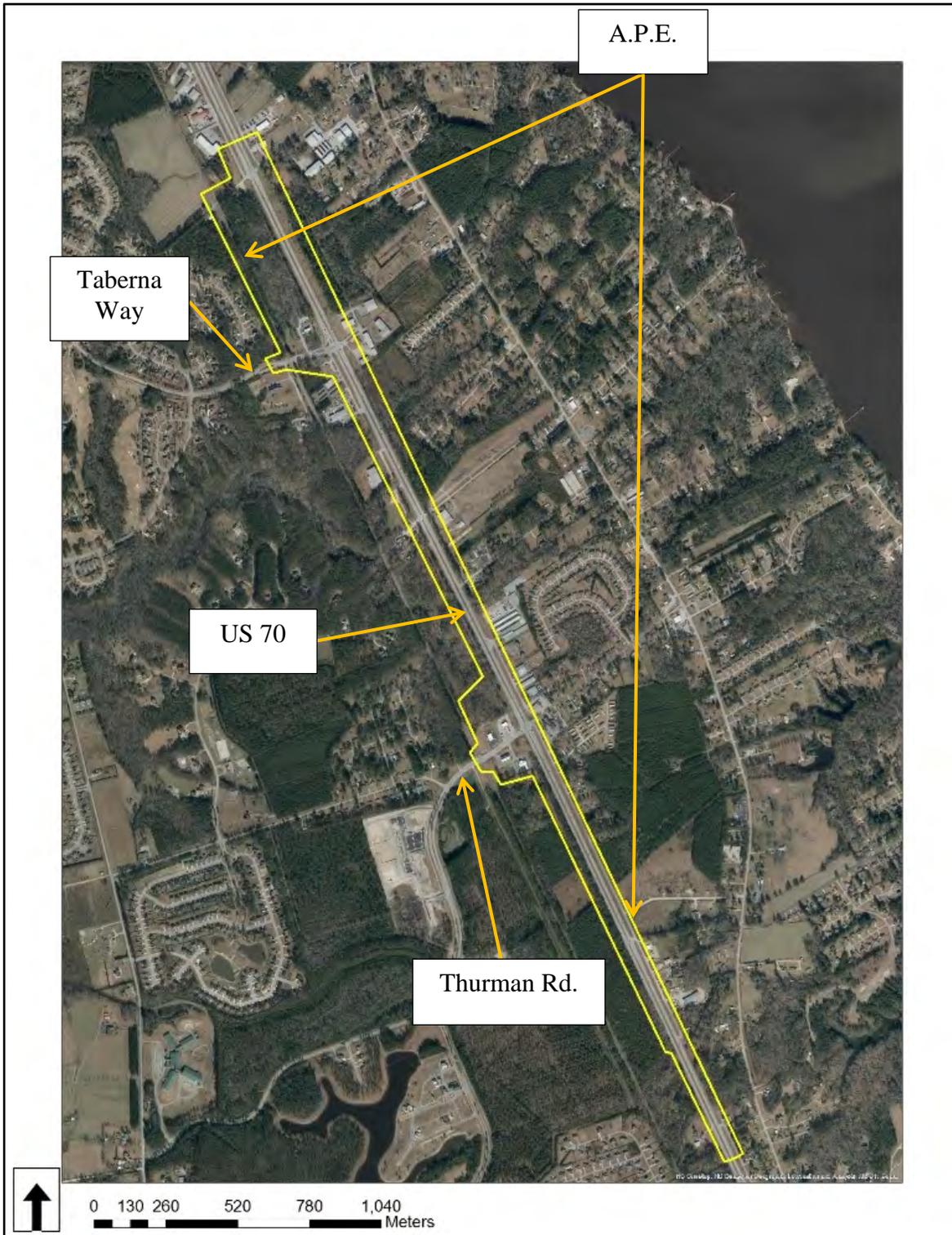
Proposed fieldwork completion date

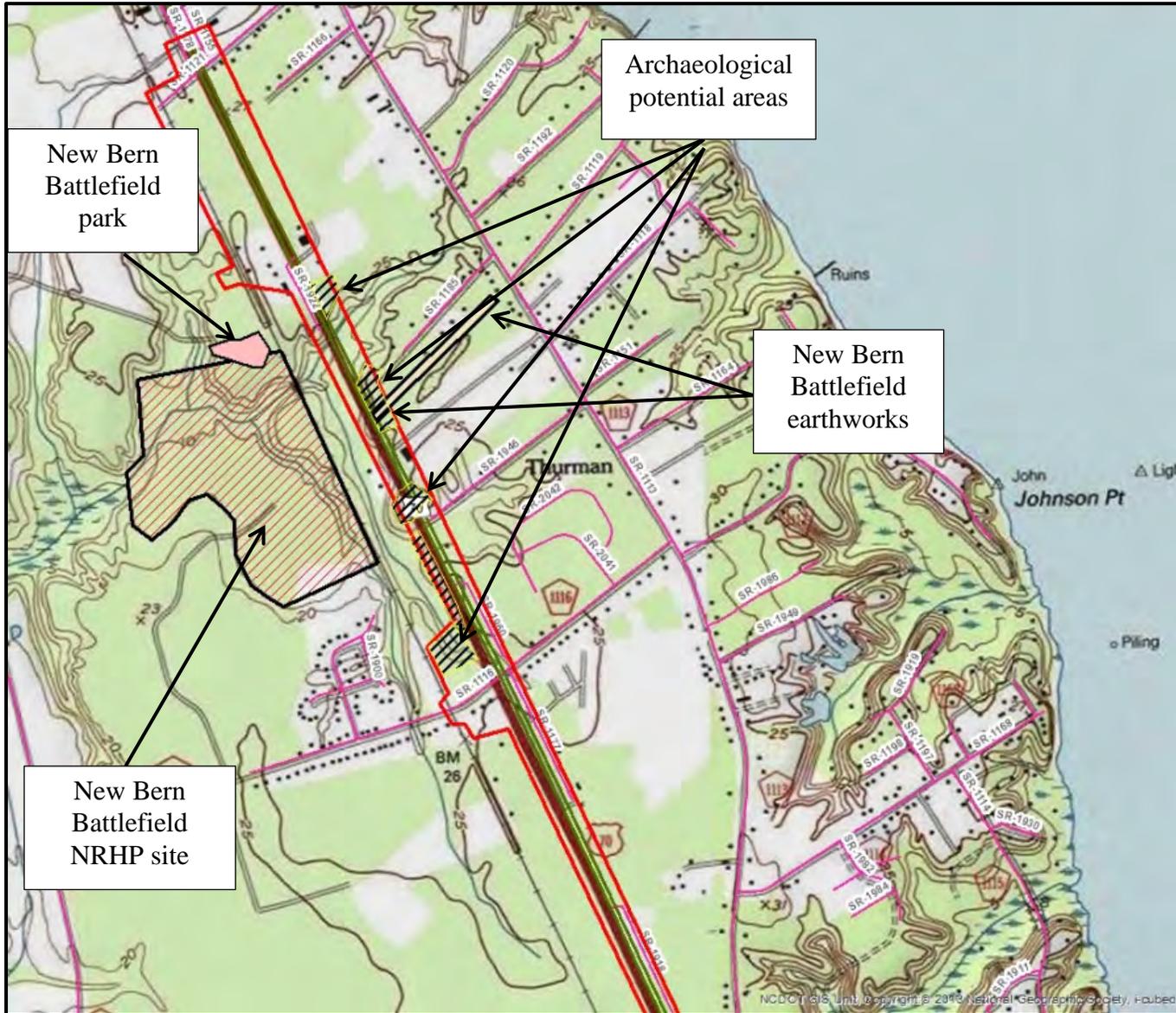














ARCHAEOLOGICAL SURVEY REQUIRED FORM

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PROJECT INFORMATION

Project No: R-5777b *County:* Craven
WBS No: 44648.1.3 *Document:* EXCLUSION
Federal Aid No: *Funding:* State Federal
Federal Permit Required? Yes No *Permit Type:*

Project Description: NOTE: This is an update of the Archaeological Survey Required form submitted on 10/23/2017.

Convert at-grade intersection of US 70 and SR 1116 (W. Thurman Rd./E. Thurman Rd.) to a restricted interchange in Craven County. Area of Potential Effects (A.P.E.) is approximately 4,103 meters (13,461 ft.) long and between 100-273 meters (328-894 ft.) wide. The A.P.E. (for this project as well as R-5777A) stretches along US 70 from Garner Rd. south to a point approximately 1,719 meters (5,638 ft.) south of SR 1116. It encompasses possible interchanges at US 70 and Taberna Way (R-5777A; see PA 17-09-0015) and US 70 and SR 1116. (This A.P.E. is longer and narrower than the October 2017 study area.) The project is State-funded, may require Federal permits, and will not require easements.

SUMMARY OF ARCHAEOLOGICAL RESOURCES REVIEW: *SURVEY REQUIRED*

Brief description of review activities, results of review, and conclusions:

The original review (October 2017) included an examination of a topographic map, the Craven County soil survey, an aerial photograph, and listings of previously recorded sites, previous archaeological surveys, and previous environmental reviews at the Office of State Archaeology (O.S.A.). US 70 is oriented approximately north-south.

The topographic map (New Bern, N.C.) shows the landforms in the A.P.E. are mostly upland flats. The A.P.E. does include the headwaters of several tributary streams of Brice Creek which is located to the west of US 70. The level, well-drained uplands adjacent to the tributaries have some potential for prehistoric archaeological sites. The uplands away from water have less potential. The October 2017 review identified five areas within the large study area that had potential for archaeological sites.

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The original study area for this project was very large in order to accommodate various alternatives for this interchange (as well as for the proposed interchange at US 70 and Taberna Way to the north [see PA 17-09-0015; R-5777A]). There were several areas with archaeological potential, and these would need to be investigated if they fell within the final design. Also, the New Bern Battlefield site would be examined if the final design impacted any areas within its boundaries.

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end of the earthworks. As mentioned above, the NRHP-listed New Bern Battlefield site is located outside of the A.P.E.

Recommend survey of the level, well-drained, undeveloped areas near water (as shown on the maps below). Determine whether the New Bern Battlefield earthworks are within the A.P.E. along the east side of US 70.

References Cited

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SUPPORT DOCUMENTATION

See attached: Map(s) Previous Survey Info Photos Correspondence
Other:

FINDING BY NCDOT ARCHAEOLOGIST – *SURVEY REQUIRED*

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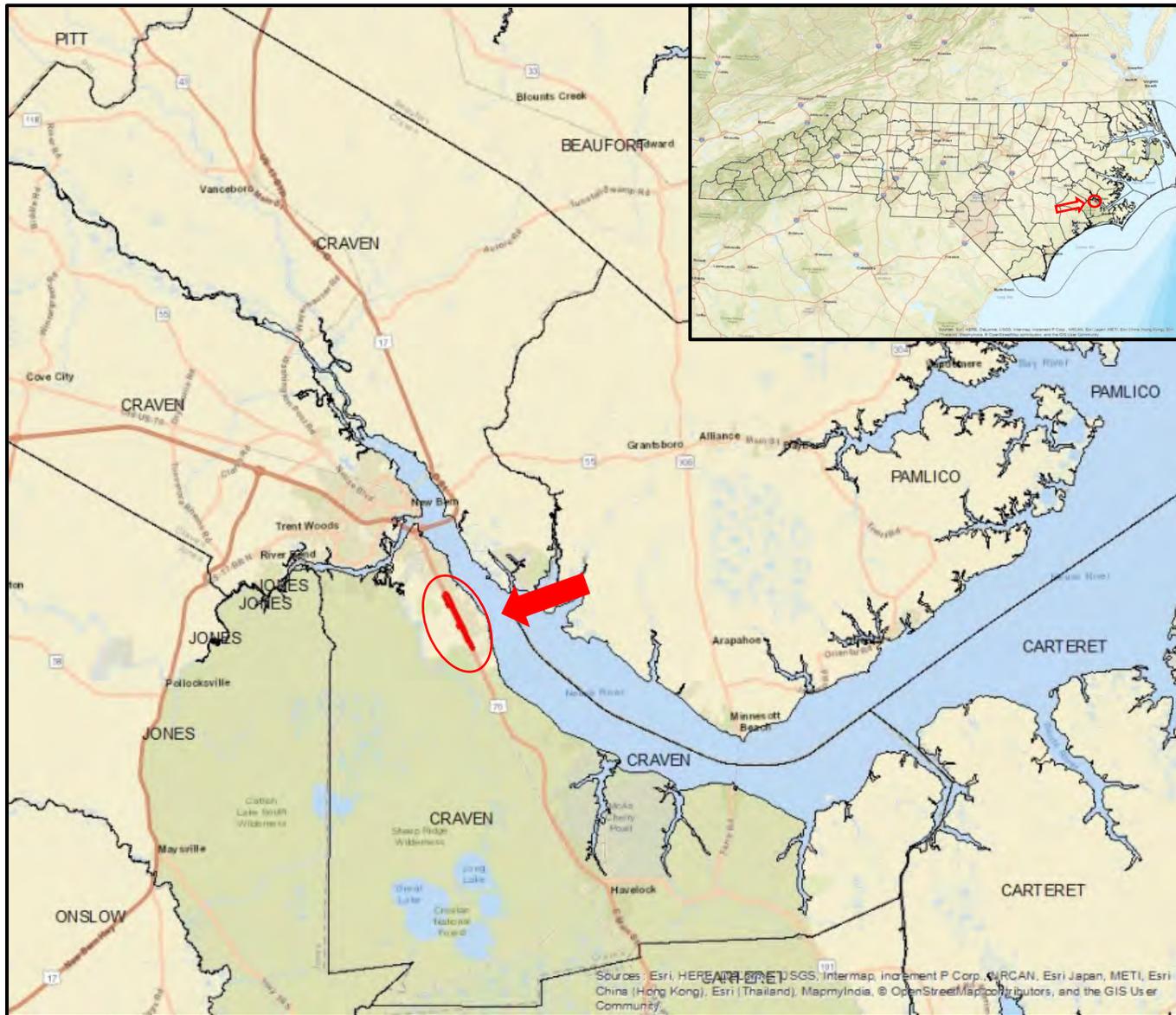
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NCDOT ARCHAEOLOGIST

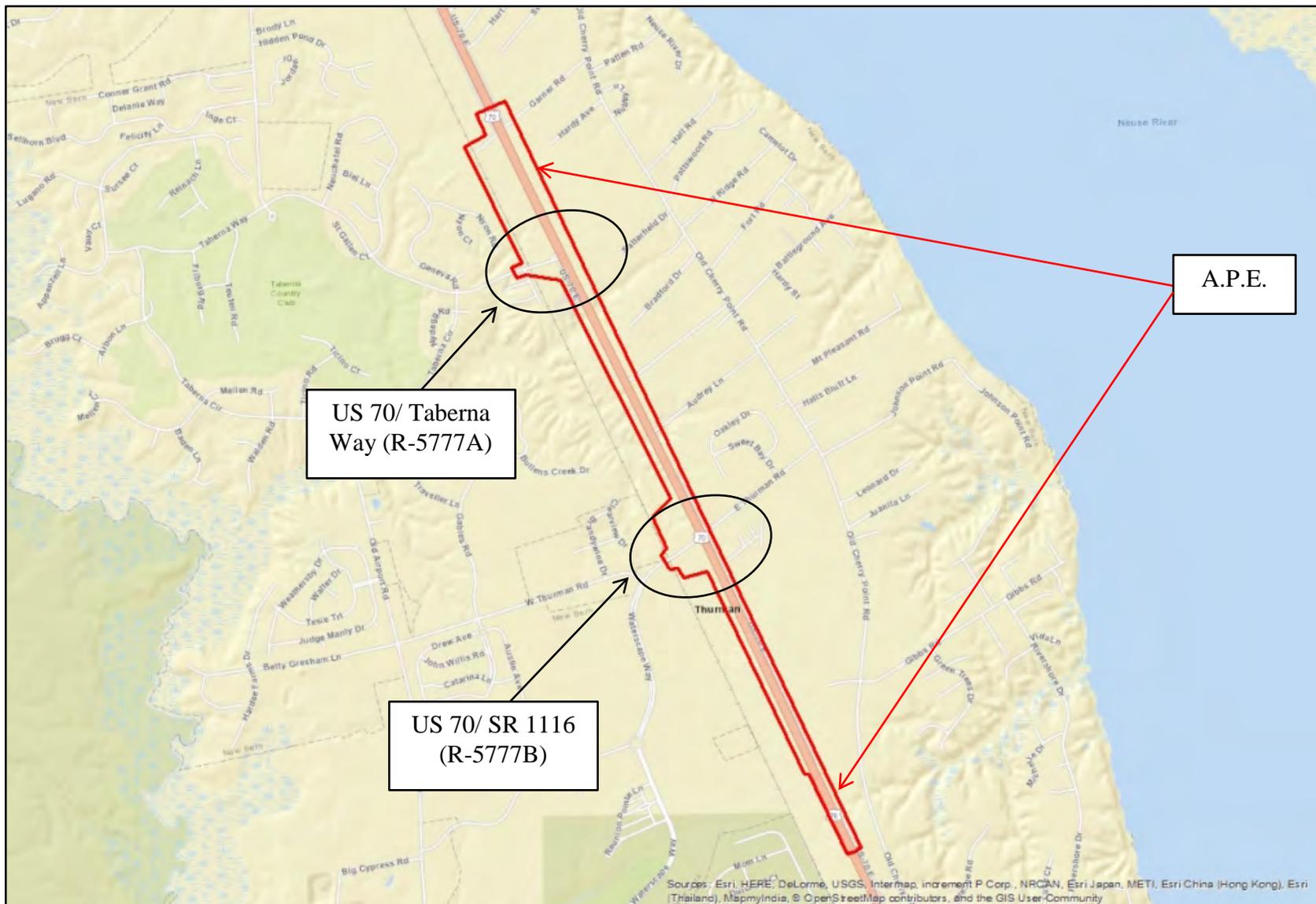
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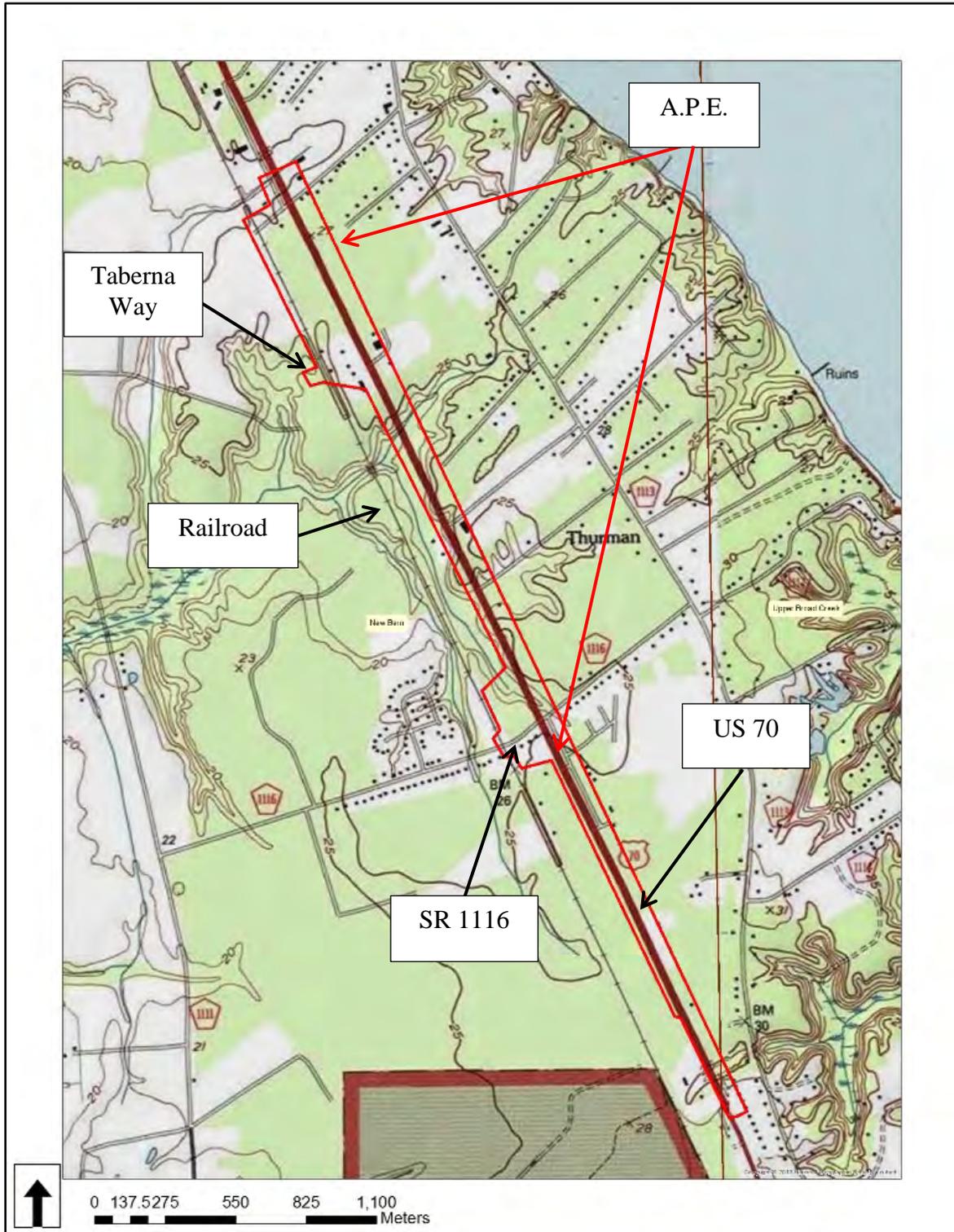
5/21/2018

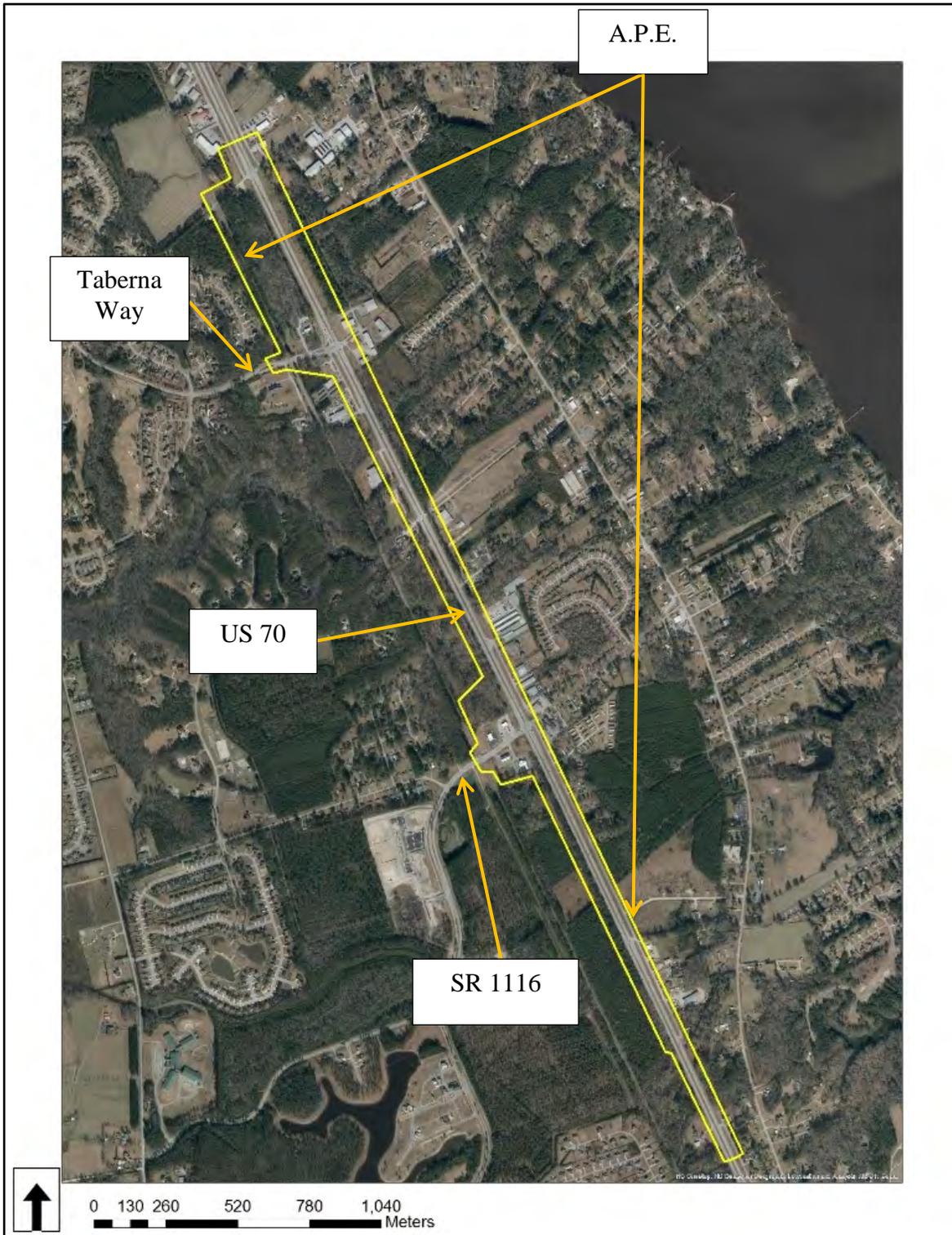
Proposed fieldwork completion date

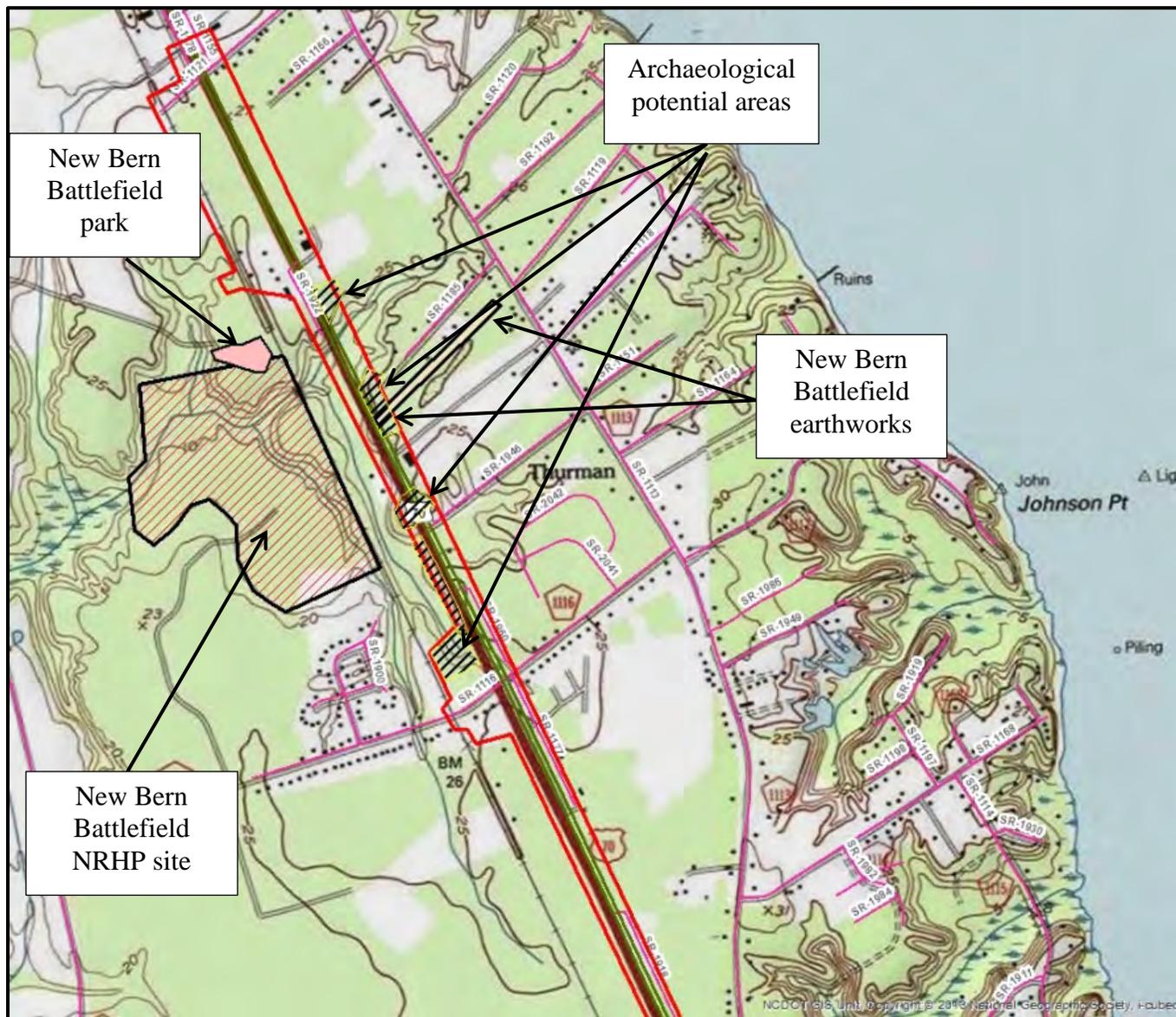


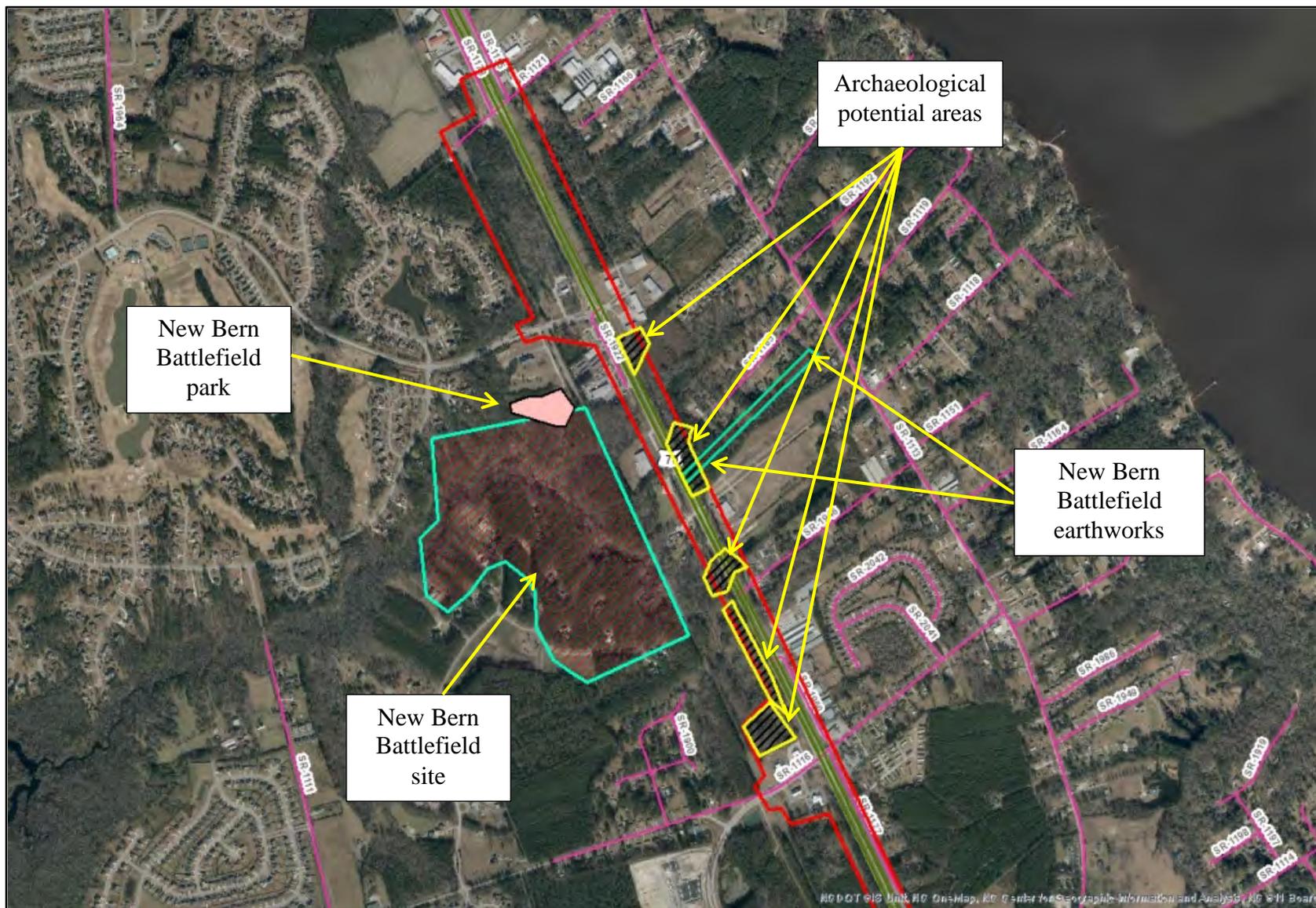












EIS RELOCATION REPORT

**North Carolina Department of Transportation
RELOCATION ASSISTANCE PROGRAM**

E.I.S. CORRIDOR DESIGN

WBS ELEMENT:	50111.1.1	COUNTY	Craven	Alternate 1 of 1 Alternate
T.I.P. No.:	U-5713			
DESCRIPTION OF PROJECT:	Upgrade US 70 to a freeway facility from SR 1124 (Garner Rd) to the Neuse River Bridge			

ESTIMATED DISPLACED					INCOME LEVEL								
Type of Displacees	Owners	Tenants	Total	Minorities	0-15M	15-25M	25-35M	35-50M	50 UP				
Residential	3	6	9	3	0	0	2	5	2				
Businesses	18	11	29	4	VALUE OF DWELLING				DSS DWELLING AVAILABLE				
Farms	0	0	0	0	Owners		Tenants		For Sale		For Rent		
Non-Profit	0	0	0	0	0-20M	0	\$ 0-150	0	0-20M	0	\$ 0-150	1	
					20-40M	0	150-250	0	20-40M	3	150-250	2	
					40-70M	1	250-400	0	40-70M	27	250-400	5	
					70-100M	0	400-600	0	70-100M	45	400-600	17	
					100 UP	2	600 UP	6	100 UP	558	600 UP	84	
					TOTAL	3		6		633		109	

ANSWER ALL QUESTIONS		
Yes	No	Explain all "YES" answers.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1. Will special relocation services be necessary?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	2. Will schools or churches be affected by displacement?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	3. Will business services still be available after project?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	4. Will any business be displaced? If so, indicate size, type, estimated number of employees, minorities, etc.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	5. Will relocation cause a housing shortage?
		6. Source for available housing (list).
<input type="checkbox"/>	<input checked="" type="checkbox"/>	7. Will additional housing programs be needed?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	8. Should Last Resort Housing be considered?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	9. Are there large, disabled, elderly, etc. families?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	10. Will public housing be needed for project?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	11. Is public housing available?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	12. Is it felt there will be adequate DSS housing available during relocation period?
<input type="checkbox"/>	<input checked="" type="checkbox"/>	13. Will there be a problem of housing within financial means?
<input checked="" type="checkbox"/>	<input type="checkbox"/>	14. Are suitable business sites available (list source).
		15. Number months estimated to complete RELOCATION? 18

REMARKS (respond by number)							
3- Business relocation will not cause a negative impact.							
4- See Attached List							
6- MLS, Newspapers, Internet, Local sources							
8- As required by law.							
11- Craven County and New Bern Public Housing Authority							
12- Or built if necessary							
14- Same as number 6 above.							

Right of Way Agent	6/11/18 Date	Relocation Coordinator	6/21/18 Date
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List of Businesses

Par.

- 033 James City Fuel Market – Small – 4-6 Employees
- 033 Triple Play – Small – 4-6 Employees
- 034 Car Wash – Small 4-6 Employees
- 040 Tidewater Appliance – Medium – 10-12 Employees
- 046 McDonalds – Medium – 12-16 Employees
- 046 Exprezit – Small – 6-8 Employees
- 050 Waffle House – Medium – 12-16 Employees
- 067 Craven Tires – Medium – 12-16 Employees
- 068 Car Tunes – Small – 4-6 Employees
- 068 Town Pawn – Small – 4-6 Employees
- 082 Riverside Sales – Small – 6-8 Employees
- 083 Riverside Subaru – Medium 12-16 Employees
- 104 Plumbing & Utility Contractors – Small – 6-8 Employees
- 122 Neuse Forest Upholstery – Small – 6-8 Employees
- 126 Terminex – Medium – 10-12 Employees
- 130 Comfort Air – Small – 6-8 Employees
- 132 Highway 55 – Small – 6-8 Employees
- 132 Vacant – Small – 4-6 Employees
- 132 Sun Tan City – Small – 6-8 Employees
- 132 Jersey Mikes – Small – 6-8 Employees
- 132 Vacant – Small – 6-8 Employees
- 132 REM Rentals – Small – 6-8 Employees
- 135 Advance Signs & Pawn – Small – 4-6 Employees
- 142 Hess – Small – 6-8 Employees
- 158 Custom Marine Fabrication – Medium – 12-14 Employees

- 167 Pamlico Marine & Trailer – Small – 6-8 Employees
- 169 Ashley Furniture – Medium – 12-16 Employees
- 175 Heavy Duty Truck – Small 6-8 Employees
- 180 J&J Automotive – Small – 6-8 Employees

EIS RELOCATION REPORT

North Carolina Department of Transportation
RELOCATION ASSISTANCE PROGRAM

E.I.S. CORRIDOR DESIGN

WBS ELEMENT:	44648.1.2	COUNTY	Craven	Alternate	of	1	Alternate
T.I.P. No.:	R-5777A&B						
DESCRIPTION OF PROJECT:	Upgrade US 70 to a freeway facility from Garner Rd to just east of E. Thurman Rd						

ESTIMATED DISPLACEDS					INCOME LEVEL								
Type of Displacees	Owners	Tenants	Total	Minorities	0-15M	15-25M	25-35M	35-50M	50 UP				
Residential	3	5	8	2	0	0	0	5	3				
Businesses	5	2	7	1	VALUE OF DWELLING				DSS DWELLING AVAILABLE				
Farms	0	0	0	0	Owners		Tenants		For Sale		For Rent		
Non-Profit	1	0	1	0	0-20M	0	\$ 0-150	0	0-20M	0	\$ 0-150	1	
ANSWER ALL QUESTIONS					20-40M	0	150-250	0	20-40M	3	150-250	2	
Yes	No	Explain all "YES" answers.											
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1. Will special relocation services be necessary?											
<input checked="" type="checkbox"/>	<input type="checkbox"/>	2. Will schools or churches be affected by displacement?											
<input checked="" type="checkbox"/>	<input type="checkbox"/>	3. Will business services still be available after project?											
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<input type="checkbox"/>	<input checked="" type="checkbox"/>	6. Source for available housing (list).											
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<input type="checkbox"/>	<input checked="" type="checkbox"/>	13. Will there be a problem of housing within financial means?											
<input checked="" type="checkbox"/>	<input type="checkbox"/>	14. Are suitable business sites available (list source).											
<input type="checkbox"/>	<input type="checkbox"/>	15. Number months estimated to complete											
					REMARKS (Respond by Number)								
					2- Brice's Creek Church – Small – 200 members 3- Business relocation will not cause a negative impact. 4- Garner Farms – Small 4-6 employees Hear Me Speak – Small 4-6 employees Therapla Junction – Small 4-6 employees United Self Storage – Small – Small 4-6 employees Mallard Convenience Store – Small 4-6 employees BB&T Medium 12-15 employees Thermik – Small 6-8 employees 6- MLS, Newspapers, Internet, Local sources 8- As required by law. 11- Craven County and New Bern Public Housing Authority 12- Or built if necessary 14- Same as number 6 above.								

Right of Way Agent	4,30,18 Date	Relocation Coordinator	05/30/2018 Date
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